Photographic Review of the Exit Hole at the Pentagon

Prepared for the August 30, 2017 9/11 Truth and Other Deep State Crimes Teleconference

> Wayne H. Coste, PE Truth Outreach, Inc.





Photographic Review of the Exit Hole at the Pentagon

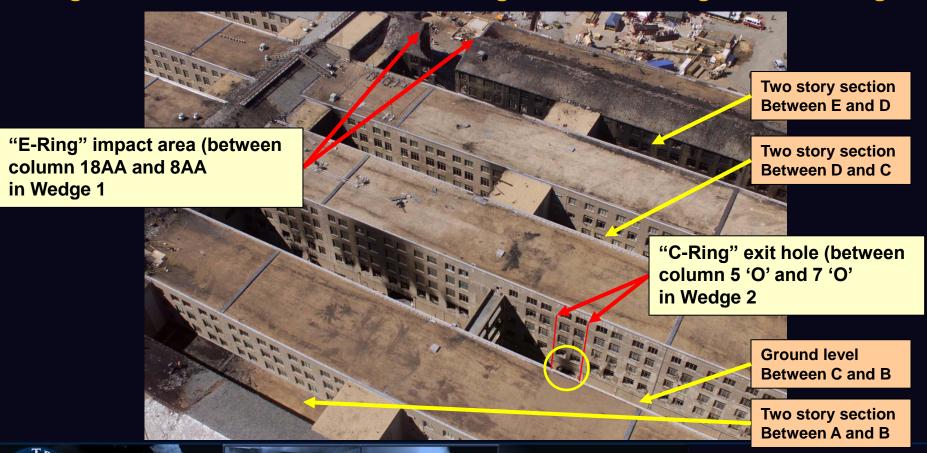
ORIENTATION





C-Ring Exit Hole is in Wedge 2

Wedge 1 rebuilt with a blast resistant design. Work on Wedge 2 had not begun.





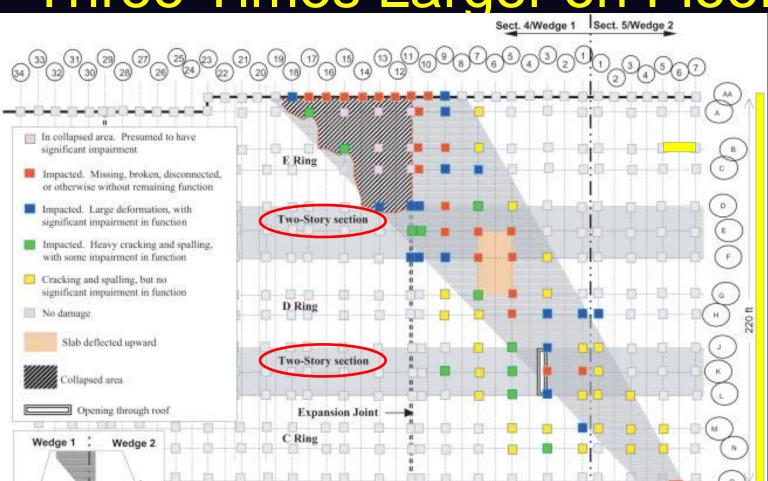


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ASCE Depicts Columns Three Times Larger on Floor Plan



Heights: Small = 0.1 Tall = 4.08 or 220 feet

At the scale shown, each column is represented as 5.4 feet square

Across the front, space between the columns appears to be slightly smaller than the width of the column shown.

ASCE report says most columns on the first floor are 21" square (or 1.75 ft)

This is 3.08 times larger than if they were drawn to scale.





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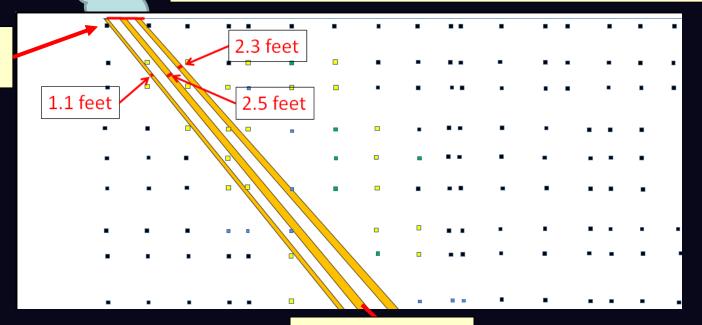
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Estimated Width of Paths (With Impaired Columns Shown)

With the column size in the ASCE Report adjusted to the correct 21 inch by 21 inch, a graphical analysis shows three paths without obstruction (see Appendix)

Debris in AE Drive continues at same angle as internal paths

"C-Ring" exit hole



To impact point





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INSIDE PHOTOS

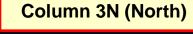




Following Photo Taken From Between Columns 3M and 3N

Angle of Photograph

Column 5N (North)



Column 3M (North)

Wedge 2

Wedge 1





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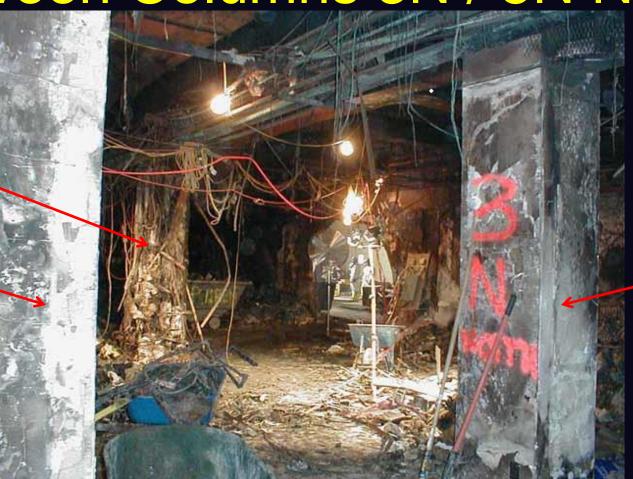
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Exit Hole From the Inside — Between Columns 3N / 5N North

Column 5N

Column 3M



Column 3N





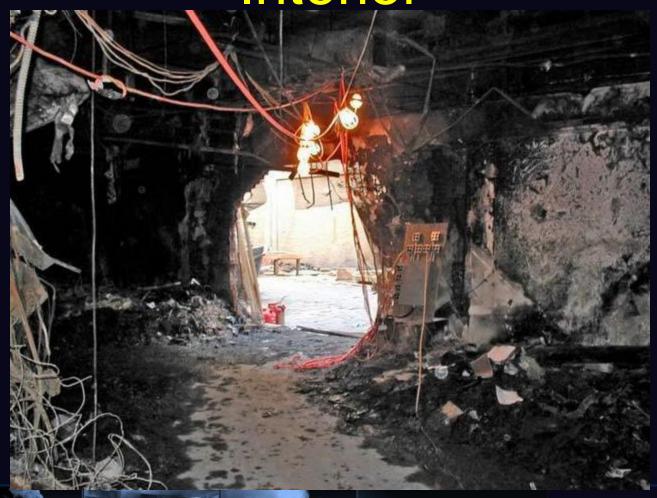
Exit Hole From the Inside — Between Columns 3N / 5N North

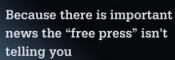






Another View of Exit Hole from Interior





Debris Path and Column Impediments to the C-Ring Exit Hole

VALIDATION OF PATH WIDTH





Validation of Path Width

- One inside the Pentagon photograph can be used for validation of the estimates
 - Taken between columns 3M and 3N (North)
 - Looking toward the C-Ring exit hole
- Objects establish relative size with certainty
 - Wheelbarrow(s) is ~20 inches wide
 - Column 3N (North) is 21 inches square





Following Photo Taken From Between Columns 3M and 3N

Angle of Photograph

Column 5N (North)

Column 3N (North)

Column 3M (North)

Wedge 2

Wedge 1



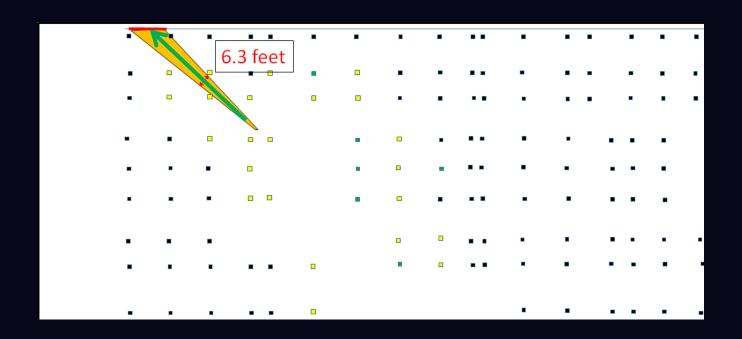


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Columns 3M and 3N Spacing Estimated at 6.3 ft





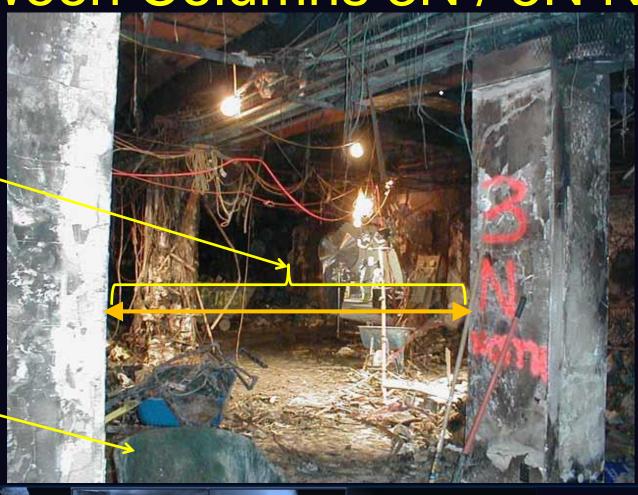


telling you

Exit Hole From the Inside – Between Columns 3N / 5N North

Estimated 6.3 foot distance

Wheel Barrow (~20")







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EXIT HOLE PHOTOS IN A&E DRIVE





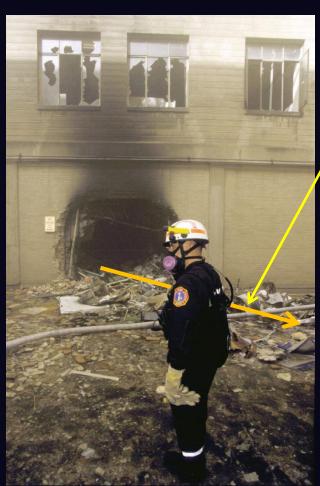
Early Photo with Smoke Rising From Exit Hole







Early Photo (Probably 9/12)



Debris path continues in AE Drive at same angle as the internal path between the columns (see Slide 5)

Internally detonated
explosives would have
created a symmetric
debris pattern - not a
debris path at same angle
as the internal path





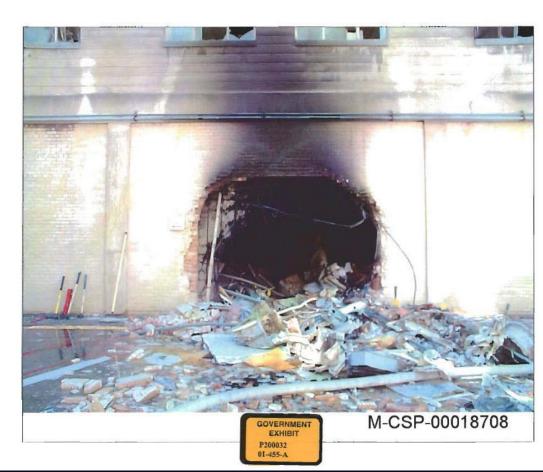


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Another Early Photo







Debris Outside of Exit Hole Many Bricks Laying Near Wall







Large Parts (With Substantial Mass) Outside Exit Hole









Large Parts (With Substantial Mass) Outside Exit Hole



Landing gear parts as reported

Tire as reported







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From "Inside the Pentagon" Clip







Firefighters Denis Griffin and Crew of Quint 104

[From the center courtyard] They carried a hose pack into Corridor 4, through the A and B Rings then turned right into the A&E Drive ...

Above them the windows were blown out, with fire spilling out. In the wall on their left they saw the three big holes, also flaming. They walked closer. At the mouth of the third hole they saw a piece of a wheel and countless shards of wreckage. Some of them looked like pieces of seats. The hole itself was almost perfectly round, big enough to stand inside. To Anderson, it was exactly the kind of hole he expected an airplane to make. Just outside the hole he saw a pair of feet. They were dark skinned, without shoes and weren't even burned. Nobody said anything as they stared at the carnage.

Source: Firefight, Inside the Battle to Save the Pentagon on 9/11, Patrick Creed and Rick Newman, 2008 (Page 148)





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Description of Debris Outside Exit Hole (page 1 of 2)

LT. COL. ERCKENBRACK

(Page 203) ... As we were moving through we crossed through the A-Ring. . . . We cleared the B-Ring and got to the breezeway between there and the C-Ring. I looked left and I saw this huge hole and [what] looked like a piece of the landing gear. The wheel and a piece of landing gear, about ten feet of landing gear had just sheered off and punched a huge hole in the C-Ring wall and had come to a stop against the backside of the B-Ring wall. Along with it there was probably a four- or five-foot-high, 10-foot wide pile of debris. As it turns out later it was like somebody had taken the plane and turned it into a syringe and everything that had been in the plane moving at 300 miles an hour had come to an immediate stop and shot forward. . . . The debris . . . was burning. It was aircraft parts, plastic, brick, people, everything. We went over there and found—I mean, went into the hole. I was the only guy with a respirator on so everybody else when they got to the hole had to stop. I went in just a little bit further and was looking for anybody that might need help. But I couldn't really see too much just because there was so much smoke and there were electrical lines that were arcing and pieces of building still falling. I got as low to the ground as I could and just kept looking around and listening. I started listening more and looking. I didn't have anything to cover my eyes and the smoke was just so acrid that you involuntarily had to close your eyes.





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Description of Debris Outside Exit Hole (page 2 of 2)

SFC. LAPIC: You were in the D-Ring?

LT. COL. ERCKENBRACK: I was in the backside of the C-Ring.

SFC. LAPIC: Okay.

LT. COL. ERCKENBRACK: I couldn't hear anything except for the electrical lines arcing.

SFC. LAPIC: You're saying the plane penetrated the C-Ring?

LT. COL. ERCKENBRACK: Yes.

SFC. LAPIC: But the landing gear was between C and D.

LT. COL. ERCKENBRACK: It was in the breezeway between the C-Ring and the B-Ring.

SFC. LAPIC: But it partially punched a hole?

LT. COL. ERCKENBRACK: Yes. It means—E, D, C. It had punched a hole through the C-Ring wall and stopped against the B-Ring wall. I listened and couldn't find anybody alive. I shimmied back out of there and we basically just looked around to see if there was anybody that was alive or needed help. We couldn't find anybody.

Source: "Then Came the Fire," http://www.history.army.mil/html/books/070/70-119-1/CMH_Pub_70-119-1.pdf





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WALL APPEARS TO BE TWO LAYERS THICK OF BRICKS





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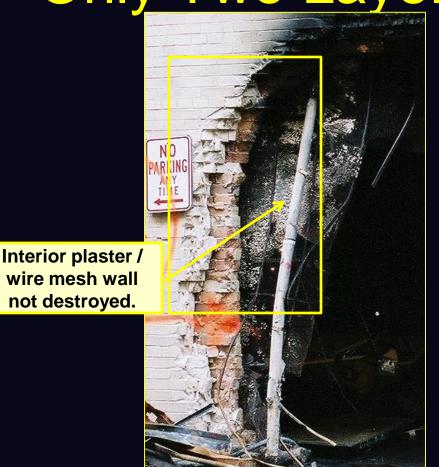
ASCE Report

- "The first story at AE Drive is brick infilled in the concrete frame, with no windows." [Page 6]
- Columns on either side of the "C-Ring Exit Hole" are approximately 20 feet apart.
 - No significant intermediary support observed between the columns
 - Between column 5 'O' and 7 'O' in Wedge 2





Wall Appears to Be Only Two Layers of Bricks Thick









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Wall Appears to Be Only Two Layers of Bricks Thick







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FEMA Urban Search and Rescue Team









FEMA Urban Search and Rescue Team









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LOOKING BACK TO IMPACT AREA



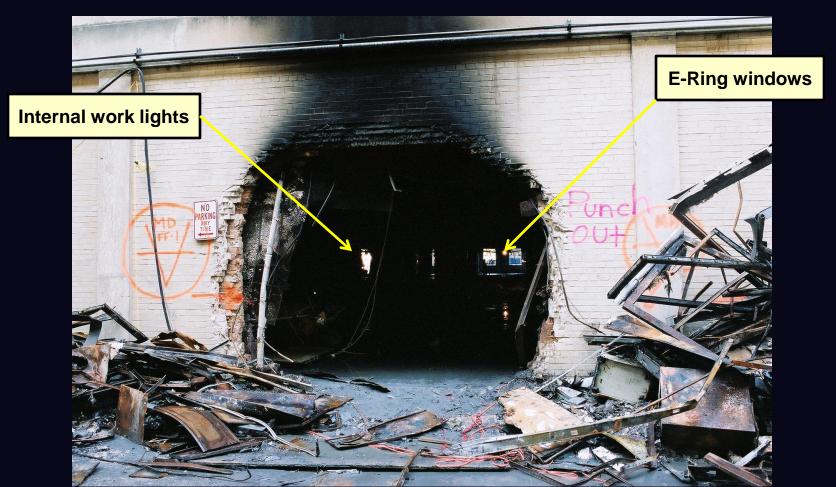


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E-Ring Windows Seen to Right







Windows Seen to Right



Windows in outer "E" Ring wall (Note: no intervening interior walls)









Debris Path and Column Impediments to the C-Ring Exit Hole

FLOOR PLANS SHOWING COLUMNS TO SCALE



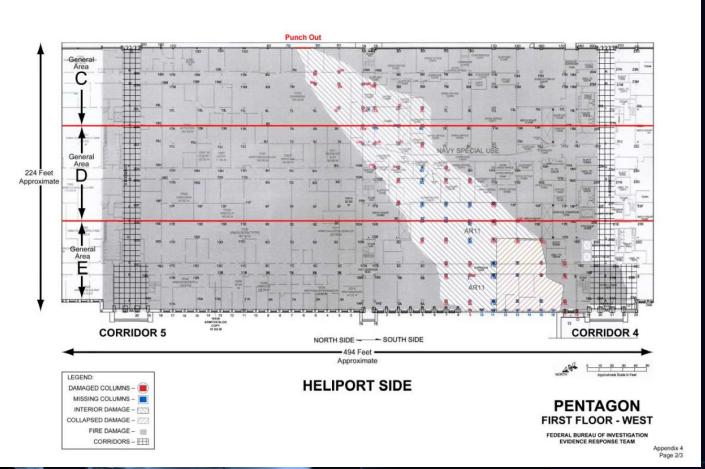


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Arlington After Action Report has Columns Drawn to Scale







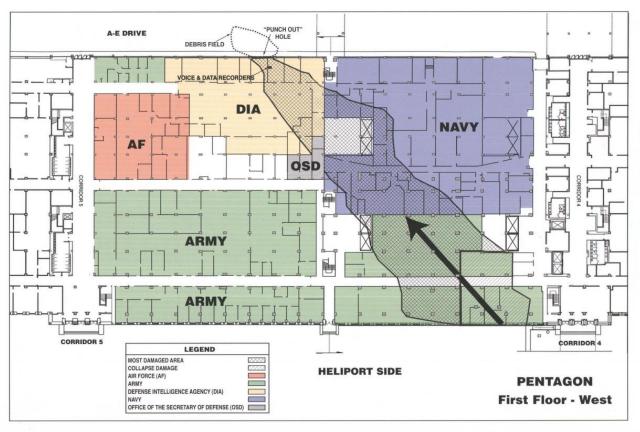
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Defense Studies Series: "Pentagon 9/11" has Columns Drawn to Scale

PATH OF AIRCRAFT THROUGH THE FIRST FLOOR







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Questions





Appendix





Debris Path and Column Impediments to the C-Ring Exit Hole

OVERVIEW OF THE INTERNAL DEBRIS PATH ISSUE





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Overview

- Prior analysis showed no clear path for debris from impact area to the exit hole
 - "Slurry of debris" flowing around columns hypothesized
 - "Slurry of debris" not a satisfactory hypothesis for the formation of the C-Ring exit hole
 - Lacks "impact" to knock down two-layer brick wall
 - Can't explain the tire / landing gear / other debris photographed outside the C-Ring exit hole

Leads to charge of "planted evidence"



Overview

- Prior analysis pf a possible path
 - Used ASCE Building Performance Report diagram for analysis
 - ASCE Report used color to indicate column status
 - Columns not drawn to scale
 - Resulting column size in image is 65" x 65"
 - Actual column size is 21" x 21"

Other floor plans show columns to scale

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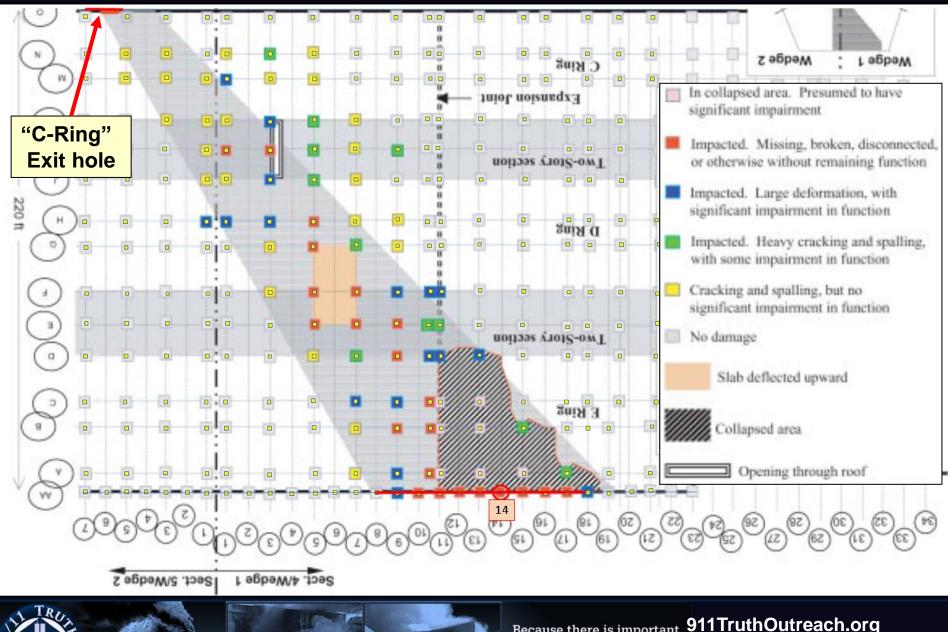
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Debris Path and Column Impediments to the C-Ring Exit Hole

ASCE REPORT





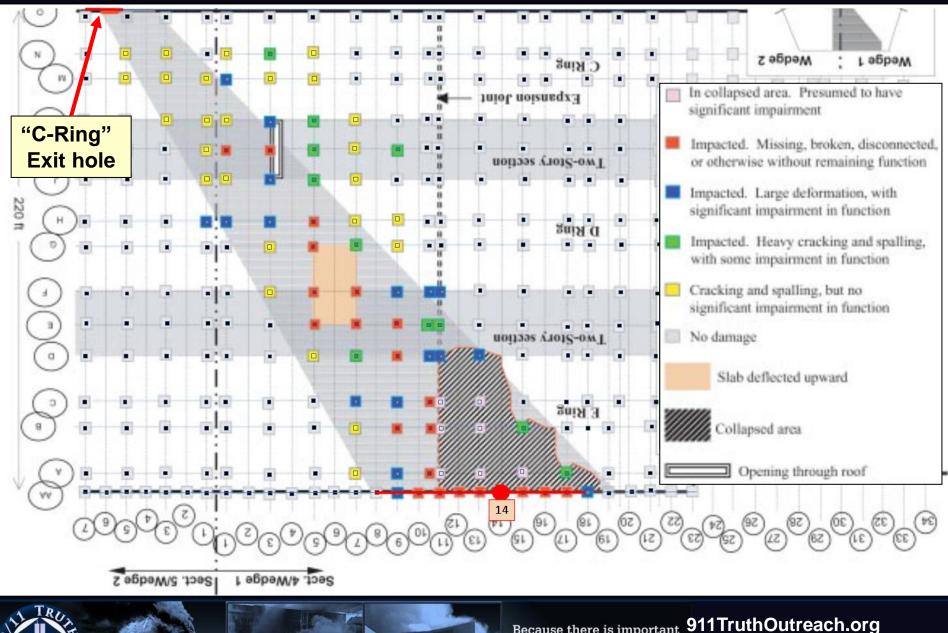






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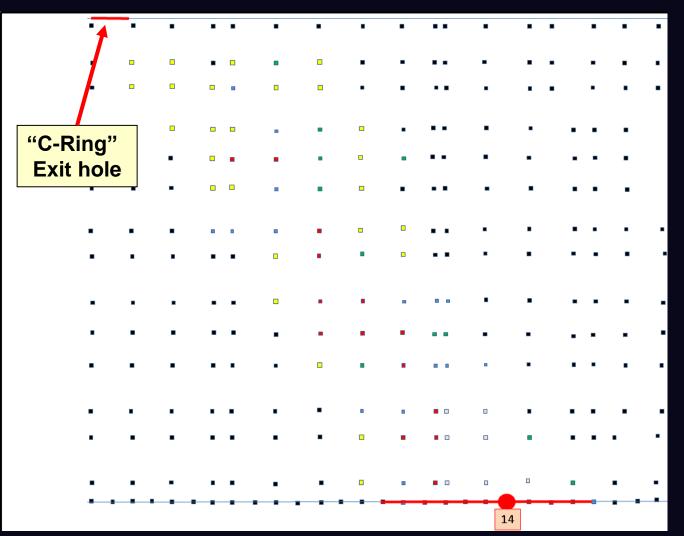






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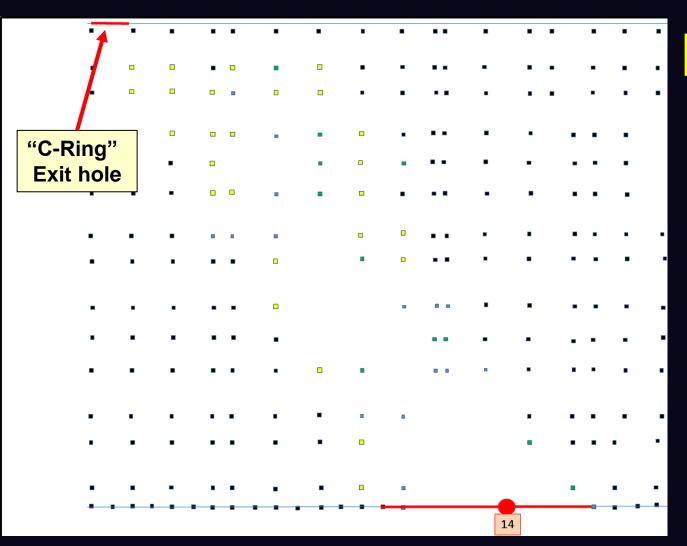


All Columns Shown

(with ASCE Colors)





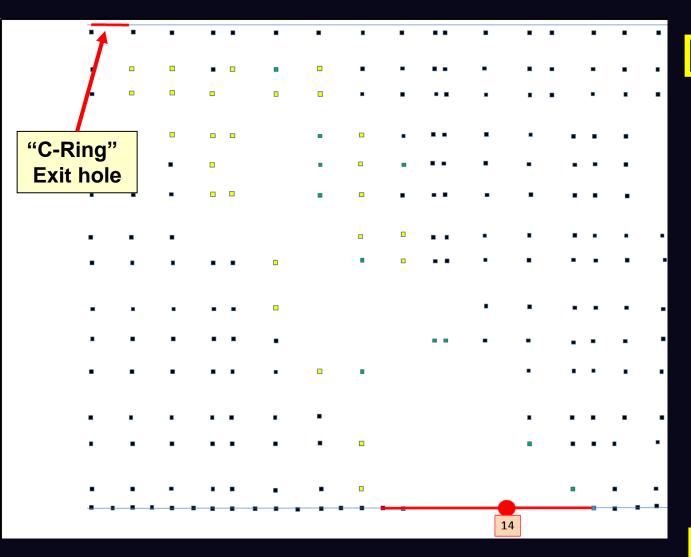


Destroyed Columns Removed

(Pink and Red)







Destroyed and Impaired Columns Removed

(Pink, Red, Blue)





Debris Path and Column Impediments to the C-Ring Exit Hole

GRAPHICAL ANALYSIS OF DEBRIS PATHWAYS

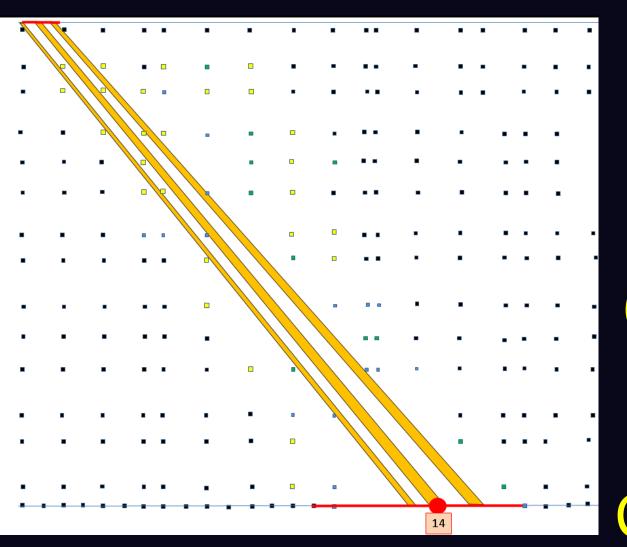




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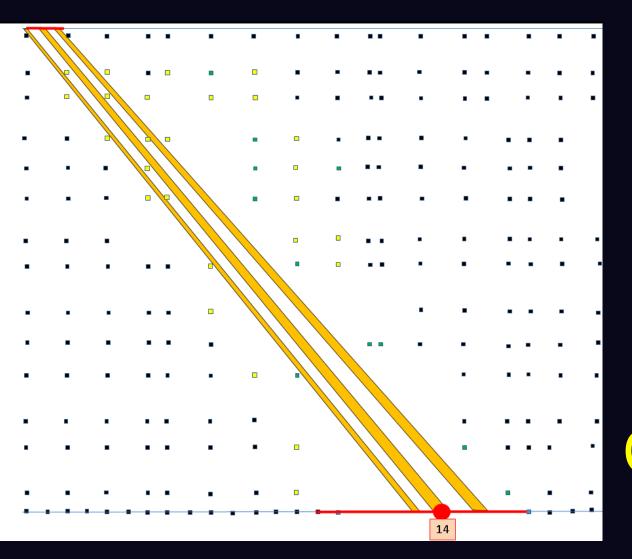


Paths Through Columns

(Includes Effect of Impaired Columns)







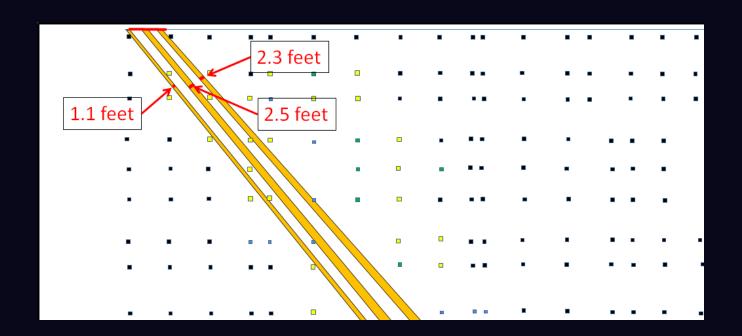
Paths Through Columns

(Without Impaired Columns)





Estimated Width of Paths (With Impaired Columns Shown)



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