

# Analysis of Pentagon Plane Approach and Impact

Prepared for the October 25, 2017  
9/11 and Other Deep State Crimes Teleconference

Wayne H. Coste, PE  
Truth Outreach, Inc.



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Table of Contents

- Orientation of aircraft approach
- “Pilots for 9/11 Truth” G-force calculation
- Plane impact into west facade
  - Approach
  - Wire spools
  - Projection of plane onto facade
    - Damage to south-end impact
    - Damage to north-end impact damage
    - Wing impact with shattered tree stump



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Question After April Presentation About Tree at Column 16

- Question after the April presentation:

*On our call, you said that you thought the tree was severed by the right wing spar.*

*Could you explain how the bottom of the right engine could hit the top of the generator trailer and then the wing dip low enough to sever the tree just a few feet from the ground?*

*And if the wing spar hit the tree, as you suggest, would the engine not gouge the lawn and/or hit some of the spools?*



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

Analysis of Pentagon Plane Approach and Impact

# ORIENTATION



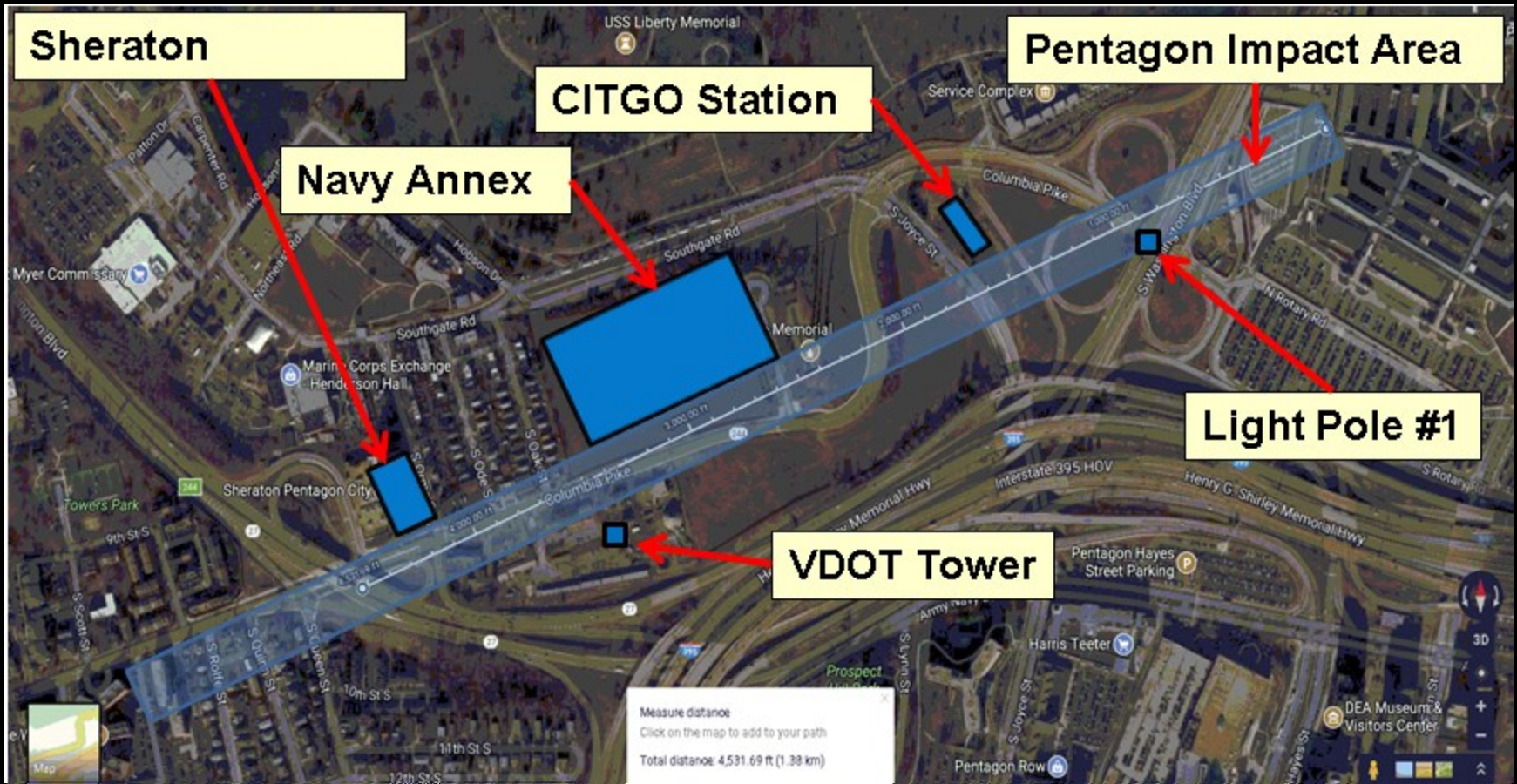
A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

**9/11 and Other Deep State Crimes Teleconference**  
**October 25, 2017**

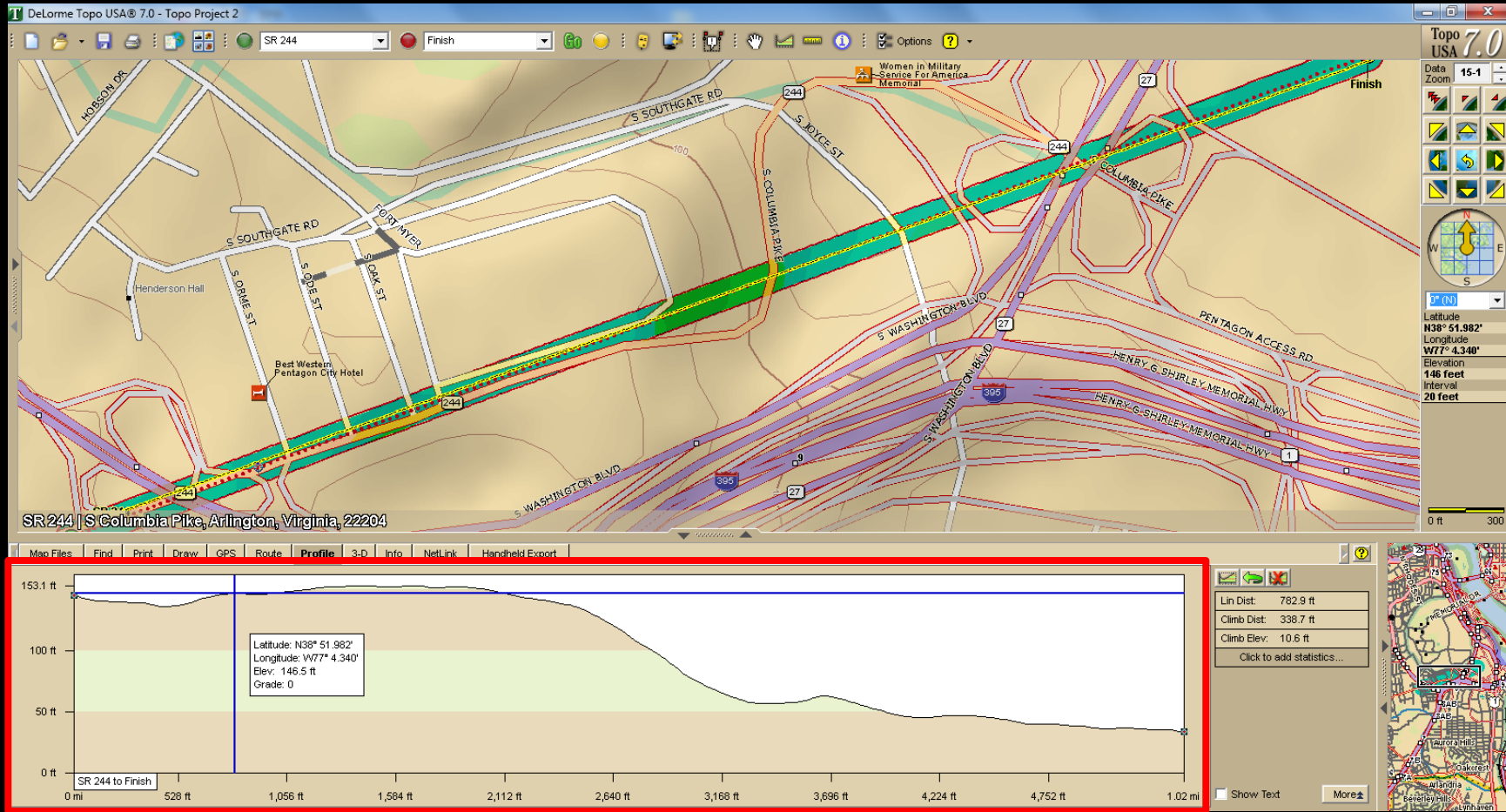
# Overview of Plane Path With Area Swept by Wings Shown



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Topography Under the Plane Path



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Topography with Obstacles Exaggerated Vertical Scale

Exaggerated Vertical Scale

Note that plane is continually descending

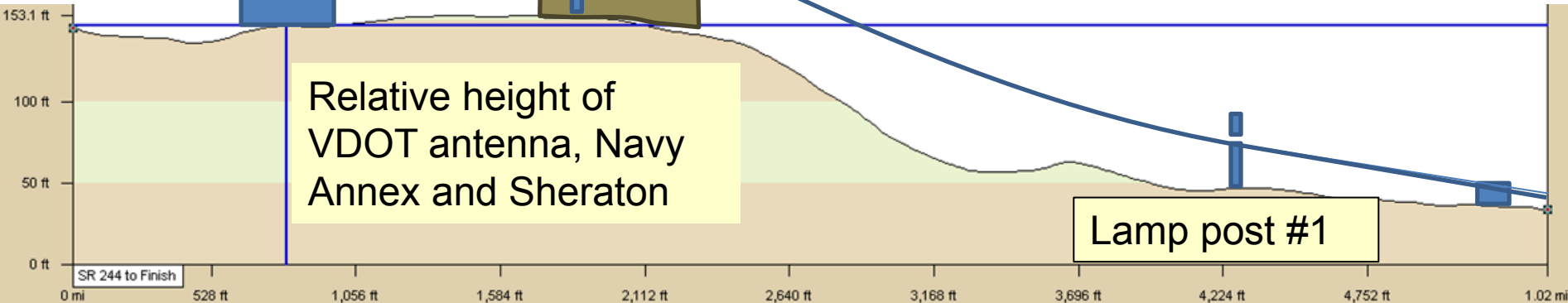
Note:  
 Deb Anlauf  
 Steve Storti  
 Dawn Vignolia  
 (and others)  
 locate plane at/  
 or below  
 Sheraton roofline

Sheraton

Navy Annex

Lamp post #1

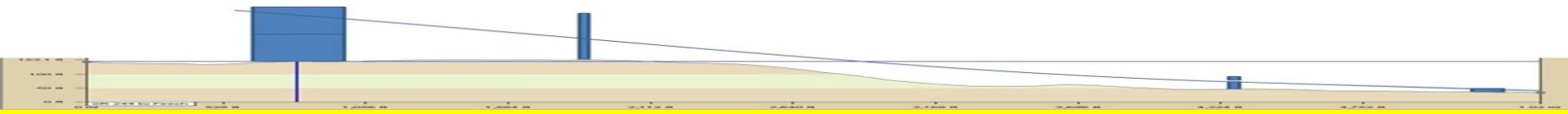
Relative height of  
 VDOT antenna, Navy  
 Annex and Sheraton



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
 October 25, 2017

# Topography with Obstacles Drawn to Scale



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017



Analysis of Pentagon Plane Approach and Impact

# APPROACHING PLANE G-FORCE CALCULATION



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Pilots for 9/11 Truth Rule-Out Plane Impact Because “High” G-Forces

- Pilots for 9/11 Truth calculate required G-Force
  - Assumes plane travels in arc with a 2,085’ radius
  - For plane pulling up out of a dive
  - Results in forces of 10.14 G
- Pilots for 9/11 Truth conclusion:

[at 9:50] ... for the least challenging "pull." If we hypothetically lower the aircraft altitude from the NTSB plotted altitude, to the lower height of the VDOT antenna. As we can see, the G-loads to “pull” out of a dive from the top of the VDOT antenna are impossible for a 757.



Because there is important news the “free press” isn’t telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

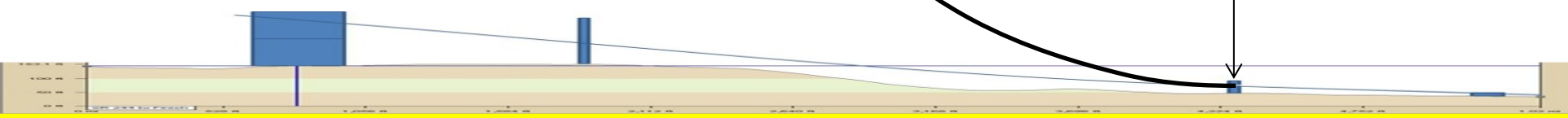
# Pilots for 9/11 Truth Calculate Upward G-Forces of 10.14G

To get their upward 10.14 G-Force, they assume the 2,085 radius arc for the approaching plane travel.

The plane must travel along this arc for their calculation to be valid.

**But the plane cannot transition into this 2,085 radius arc from the path from Sheraton or VDOT.**

2,085 ft radius



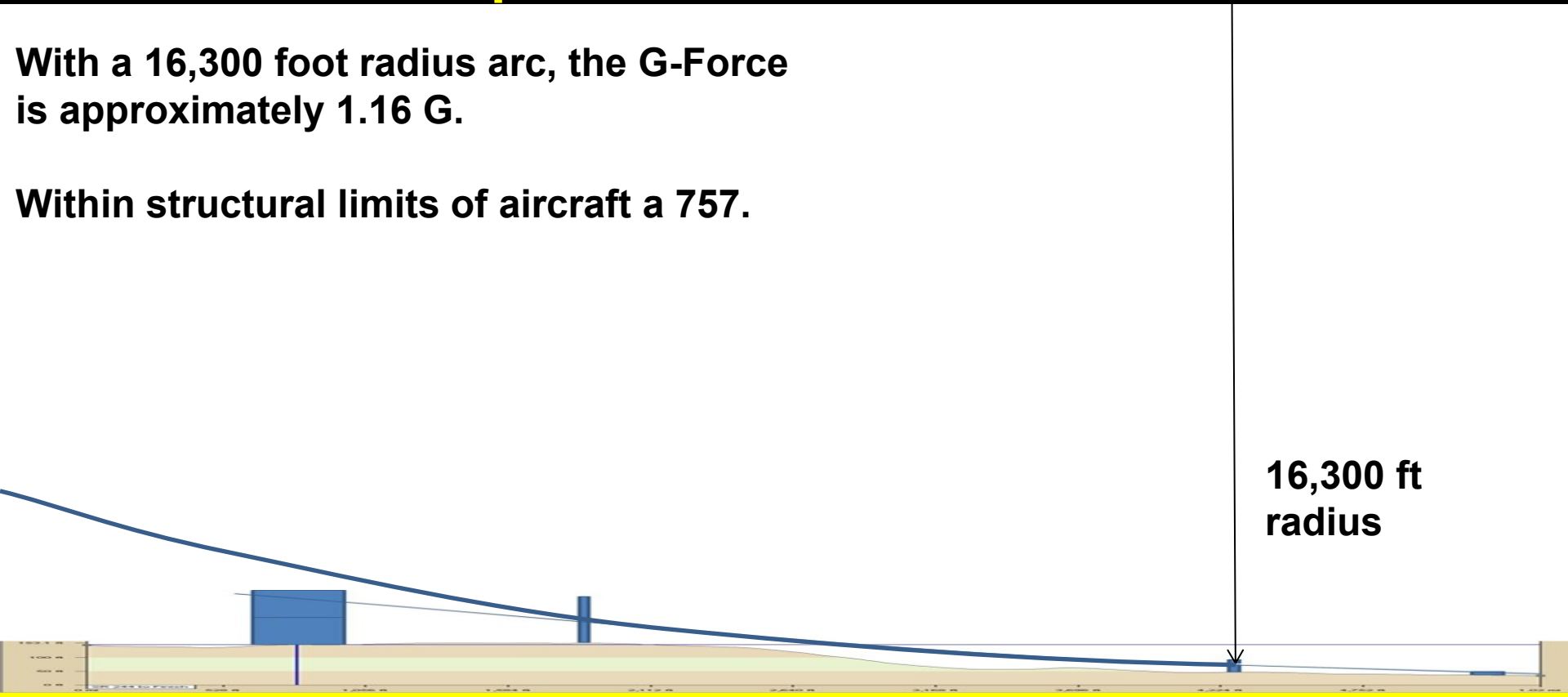
Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# 16,300 foot Radius Arc Creates Upward G-Forces of 1.16G

With a 16,300 foot radius arc, the G-Force is approximately 1.16 G.

Within structural limits of aircraft a 757.



16,300 ft  
radius



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

Analysis of Pentagon Plane Approach and Impact

# APPROACHING PLANE NEAR IMPACT OVERVIEW



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# West Wall of the Pentagon Before 9/11



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Retaining Wall Gouge

Photo Taken Approximately Perpendicular to Column 14



Impacted retaining wall gouge



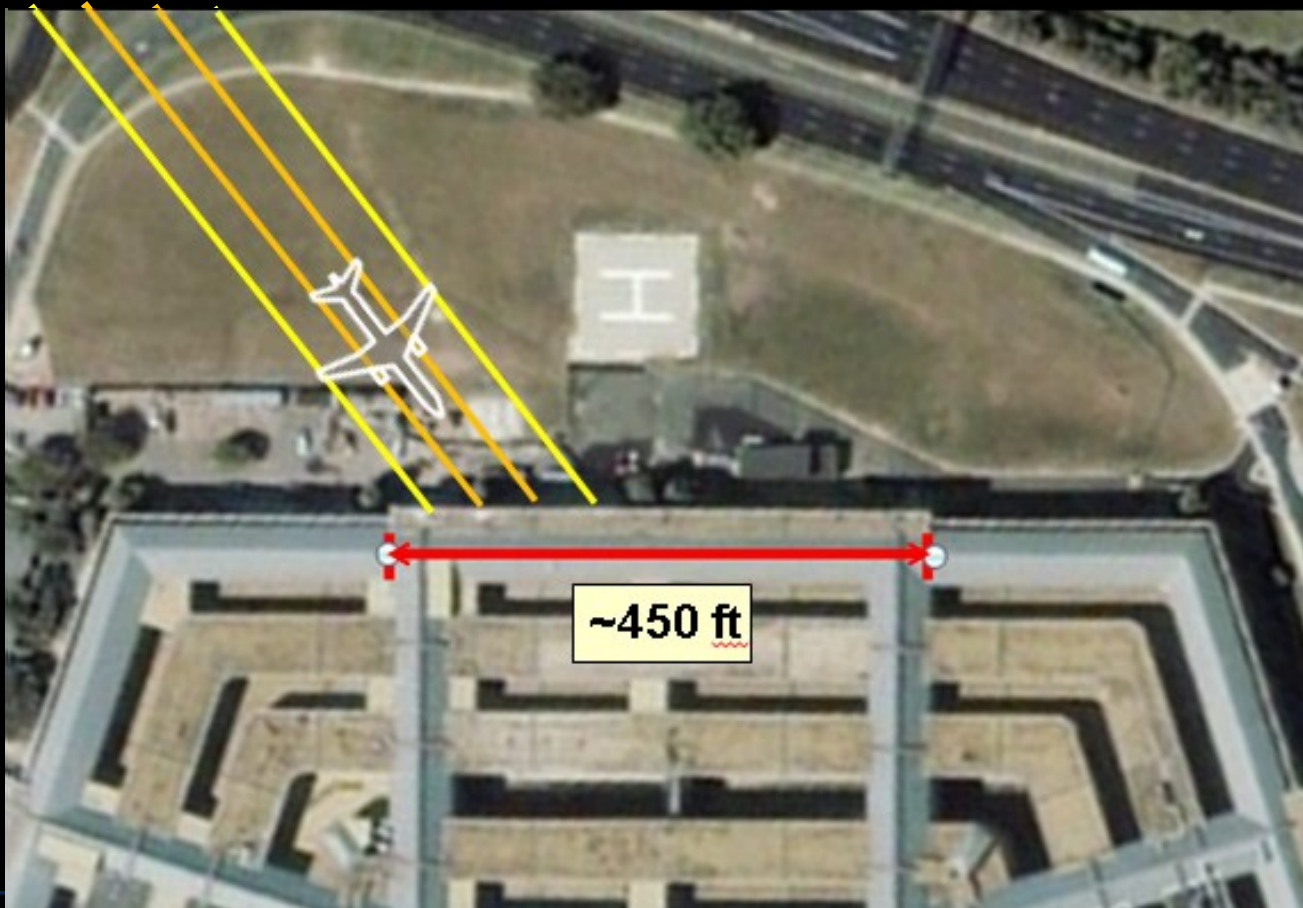
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Dimensions Relevant to Approaching Aircraft



A 501c(3) Non-Profit Educational Organization

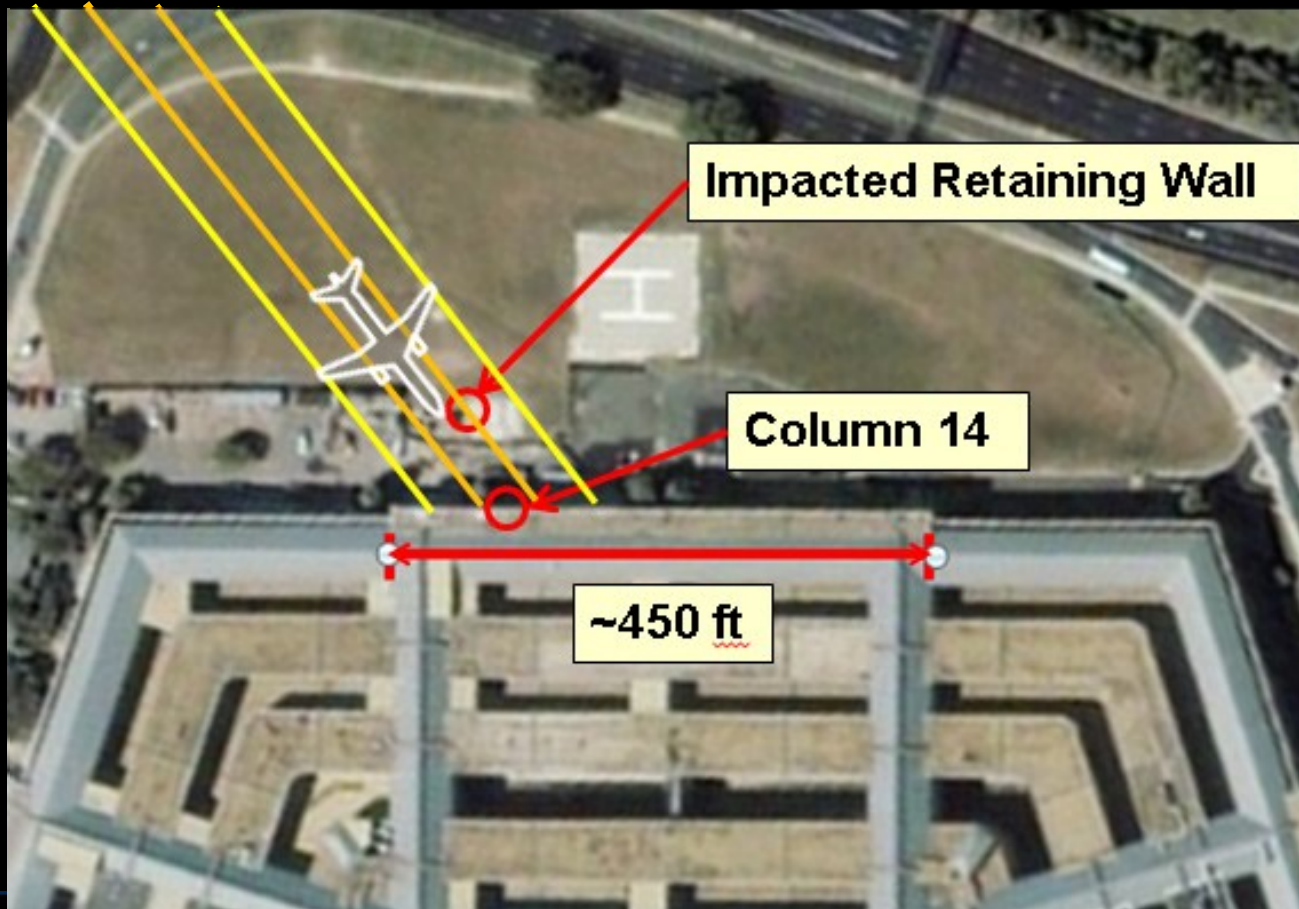


Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017



# Dimensions Relevant to Approaching Aircraft



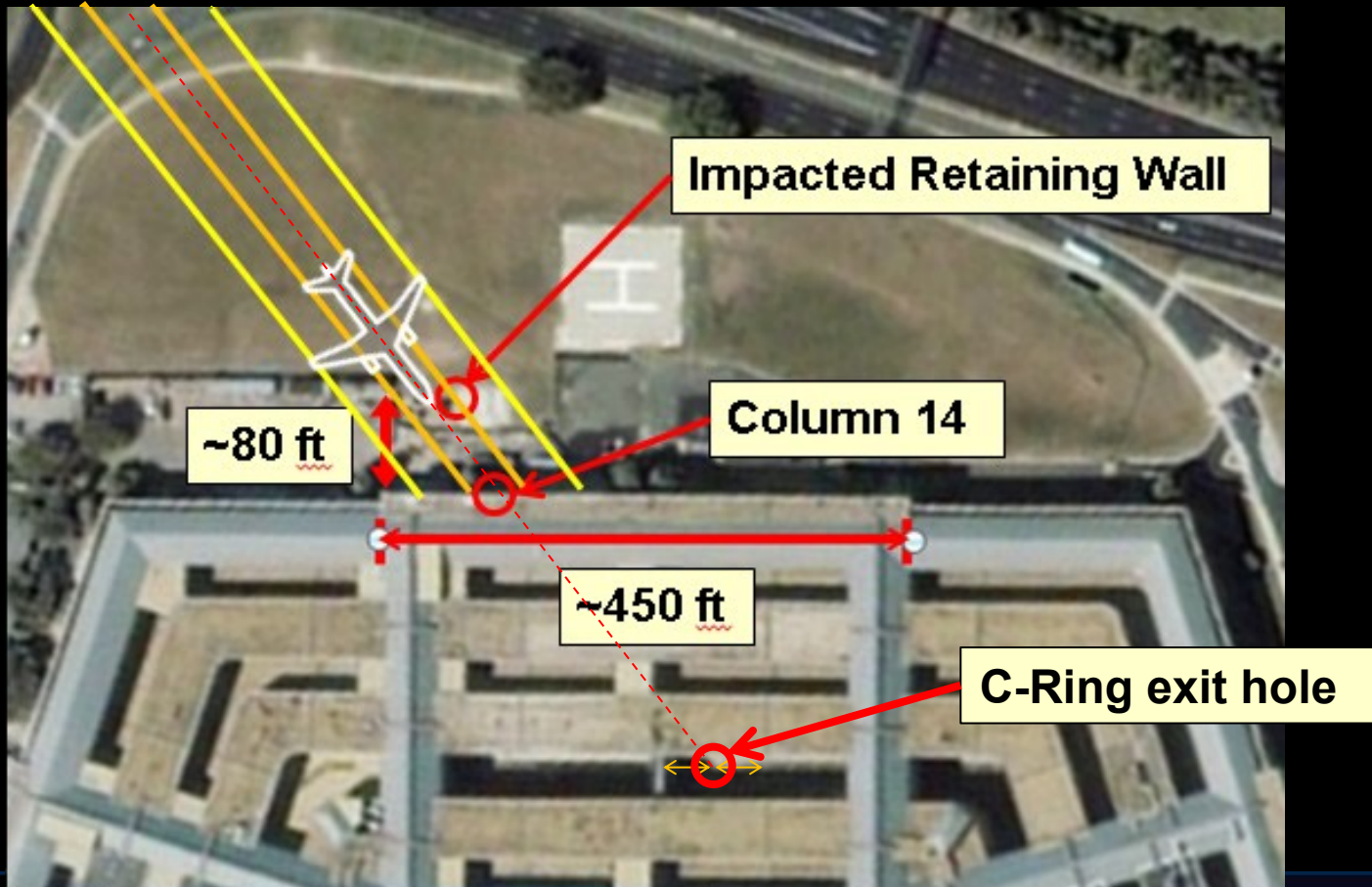
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

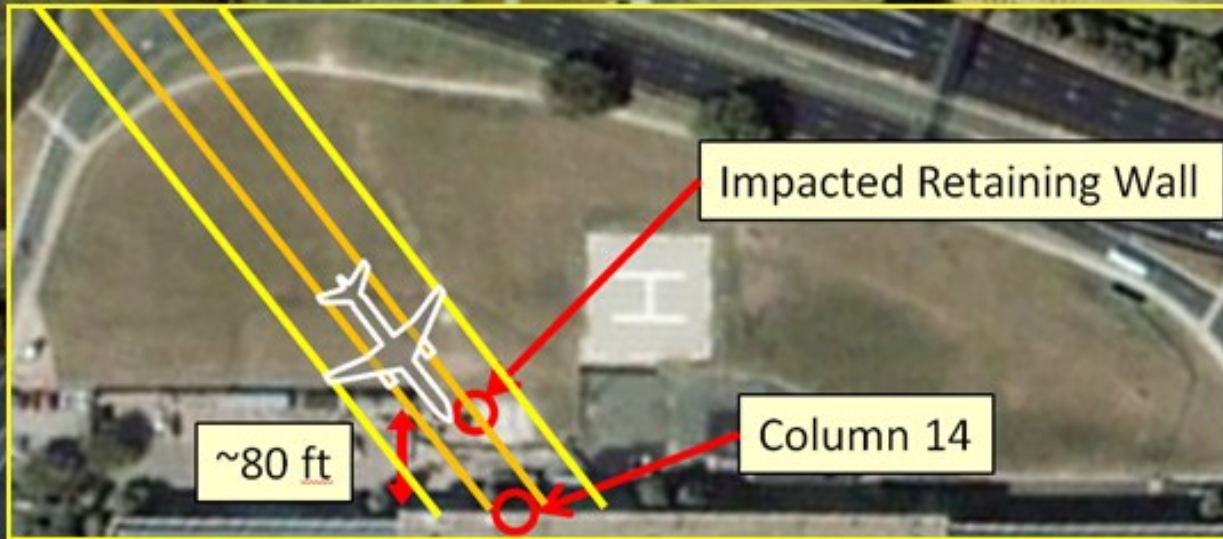
# Dimensions Relevant to Approaching Aircraft



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Plane Superimposed Above Lawn



Note: Plane width scaled by  $0.866 = \cos(30 \text{ deg})$  to reflect 30 degree perspective.



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 Truth and Other Deep State Crimes Teleconference 19  
October 25, 2017

Analysis of Pentagon Plane Approach and Impact

# WIRE SPOOLS



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference 20  
October 25, 2017

# Wire Spools

- Presence of wire spools have been cited as evidence against a large plane impact
  - Post-impact photos show them in path of plane
  - However, initial location not previously known
- Now, sufficient pre-impact photos are available to document initial location of seven spools
  - Five are seen in post-impact photos
    - Spools #5 and #6 are probably labeled correctly
    - Spools labeled #3, #4 and #7 are “best guess”



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# West Wall of the Pentagon Before 9/11 – Wire Spools



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Zoom-in of Wire Spools



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Zoom-in of Wire Spools With Spools Labeled



A 501c(3) Non-Profit Educational Organization

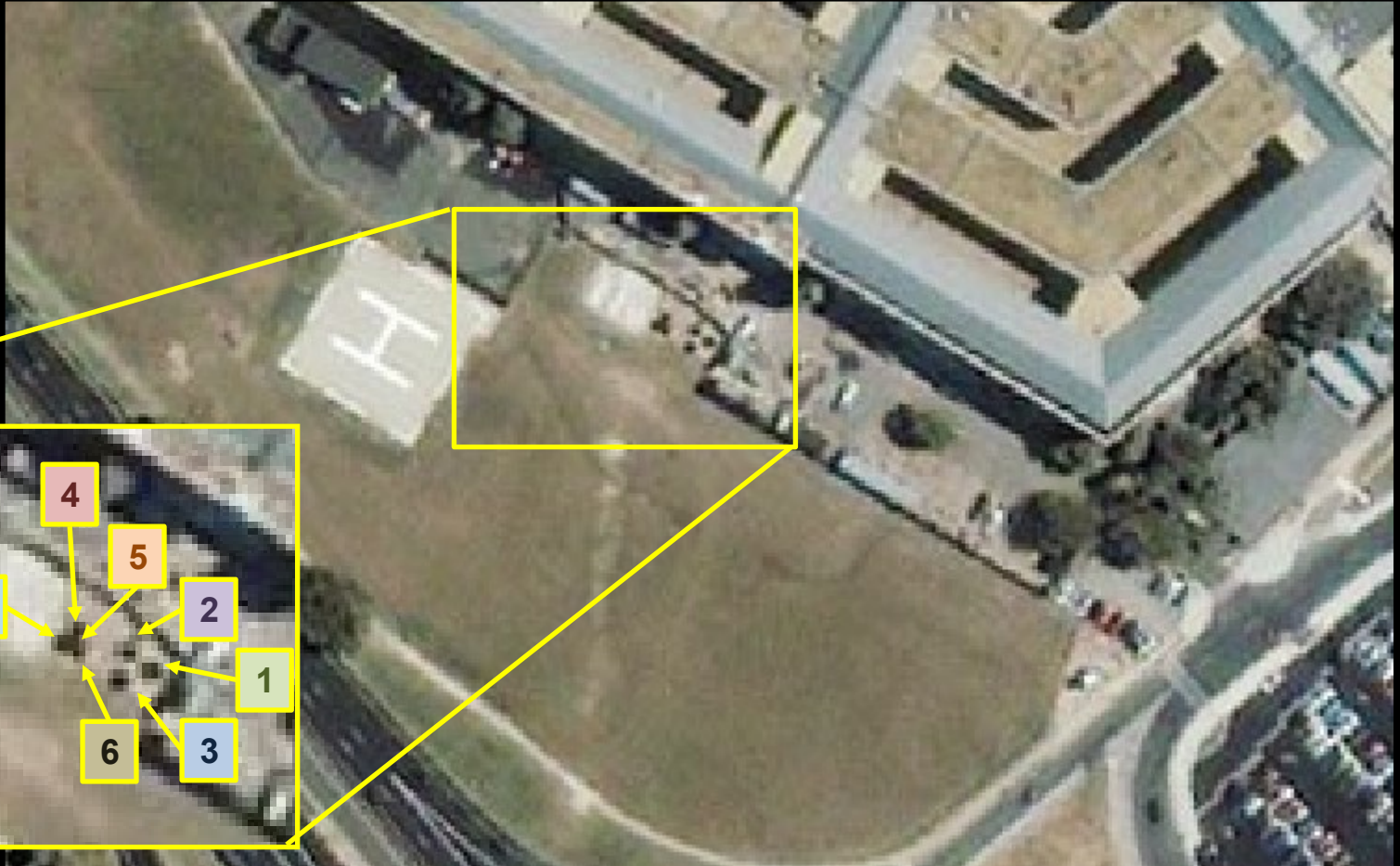


Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017



# Sept 7<sup>th</sup> Satellite Photograph Supports Spool Location



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Five of Seven Spools Are Seen Post-Impact



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

**9/11 and Other Deep State Crimes Teleconference**  
October 25, 2017

# Five of Seven Spools Are Seen Post-Impact With Spools Labeled



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Three Spools Knocked Toward Wall



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

**9/11 and Other Deep State Crimes Teleconference**  
October 25, 2017

# Three Spools Knocked Toward Wall With Spools Labeled



Because there is important news the “free press” isn’t telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Three Spools Knocked Toward Wall Fifth Spool Obscured by Smoke



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Three Spools Knocked Toward Wall With Spools Labeled



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

Analysis of Pentagon Plane Approach and Impact

# IMPACT SEQUENCE ILLUSTRATING YAW MOTION



A 501c(3) Non-Profit Educational Organization

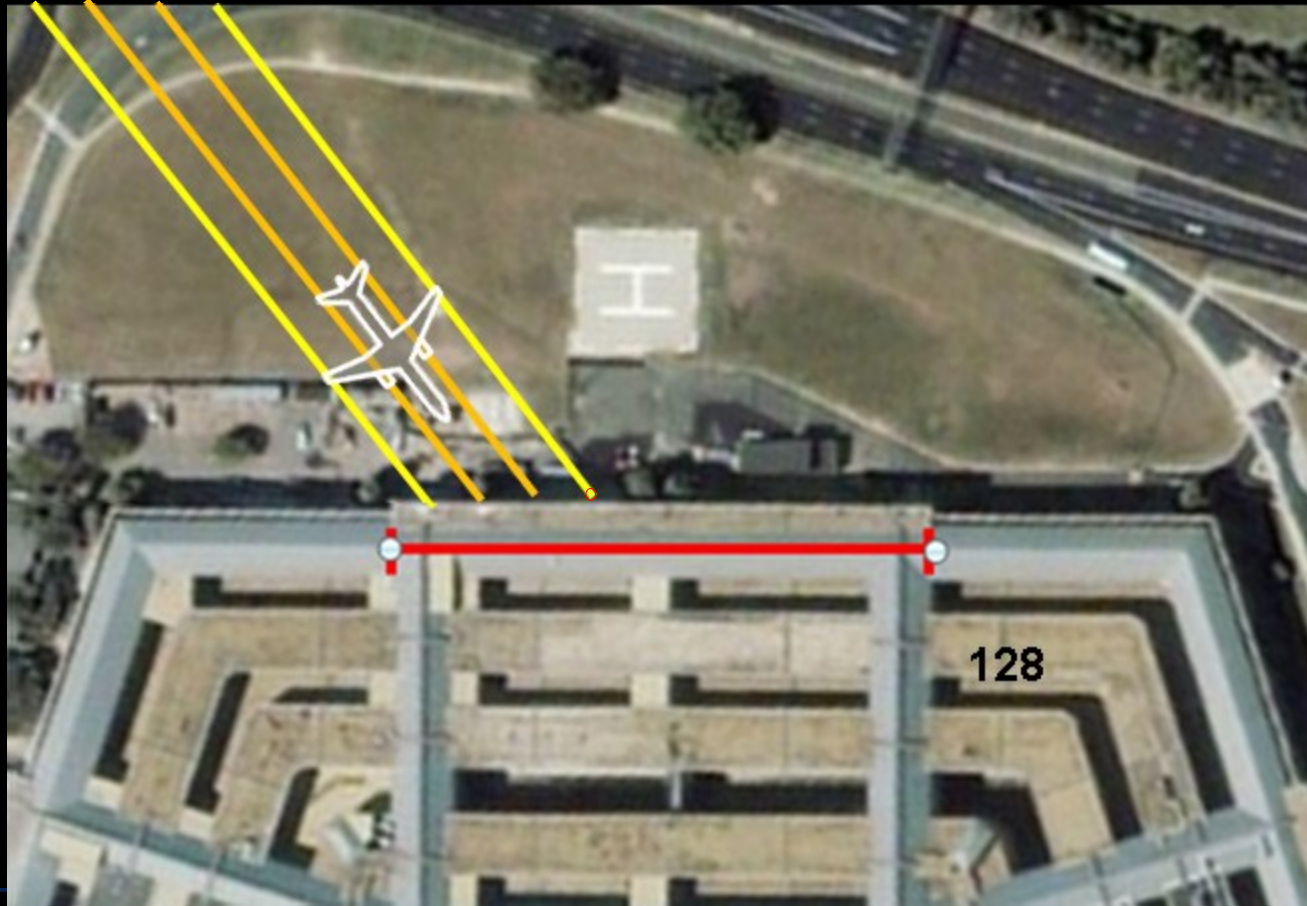


Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017



# Before Hitting Generator Trailer



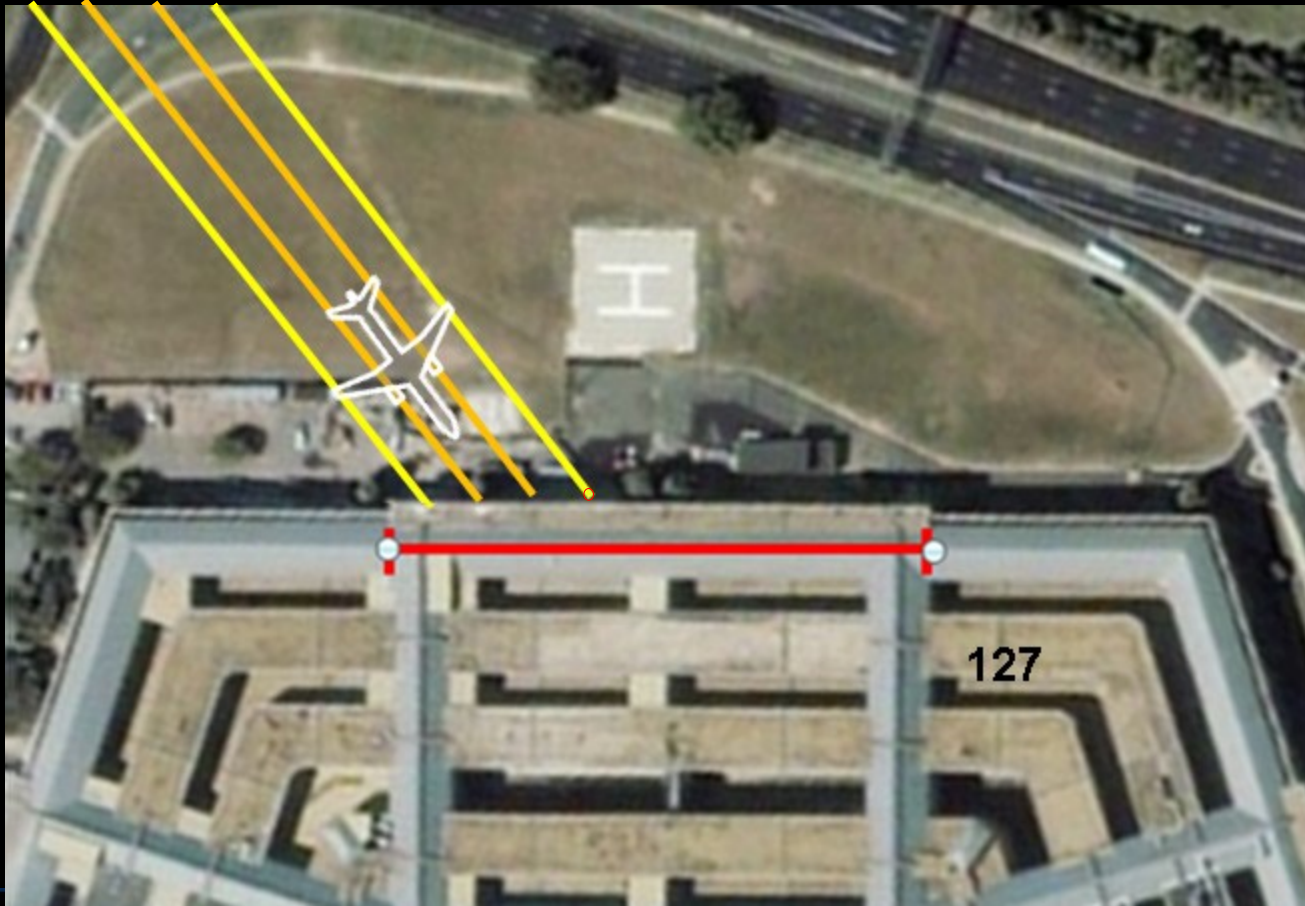
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Hits Generator Trailer Yaw Rotation Begins



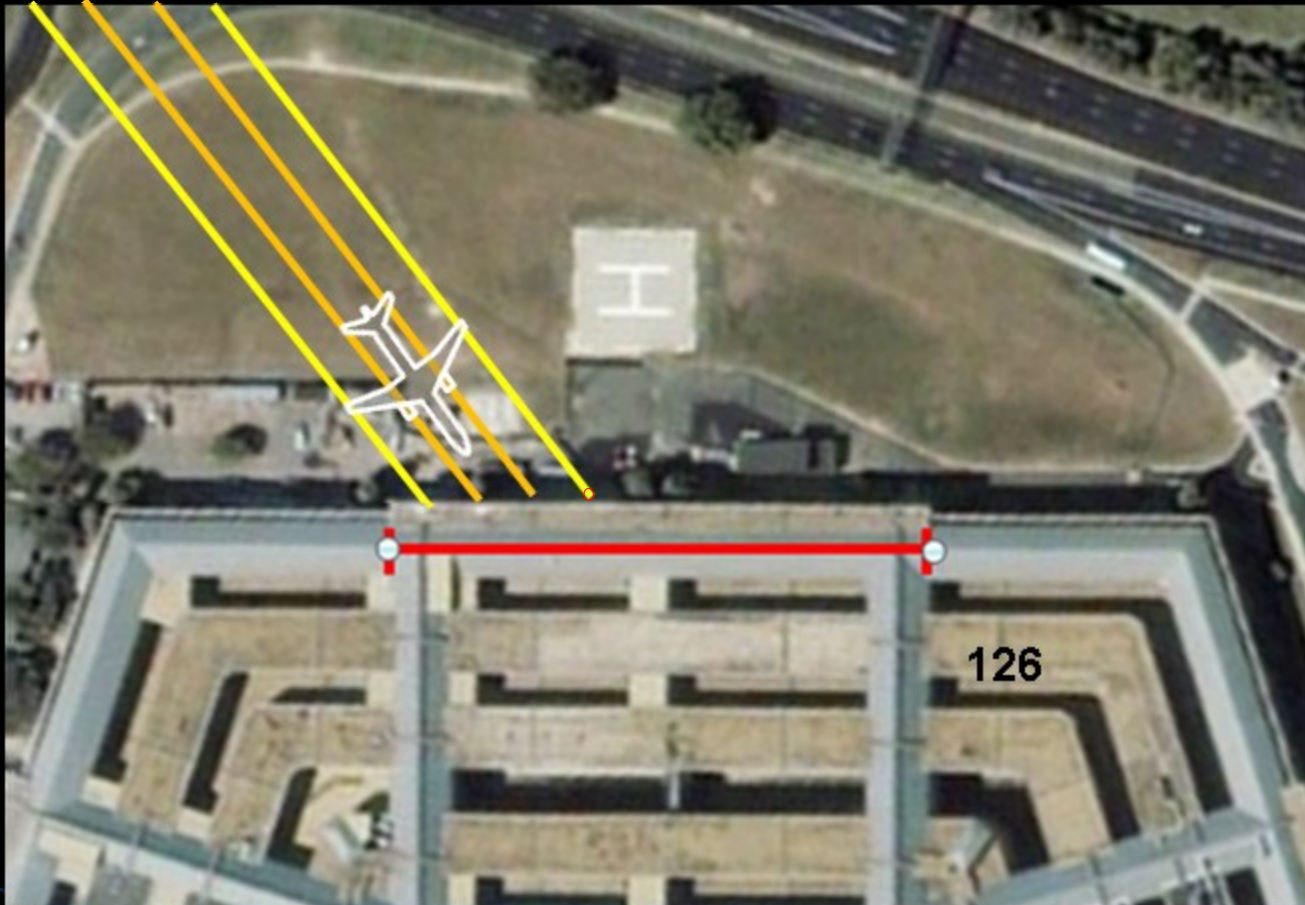
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Before Hitting Retaining Wall Yaw Rotation Continues



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Left Engine Impacts Retaining Wall Right Engine Clears Gen Trailer



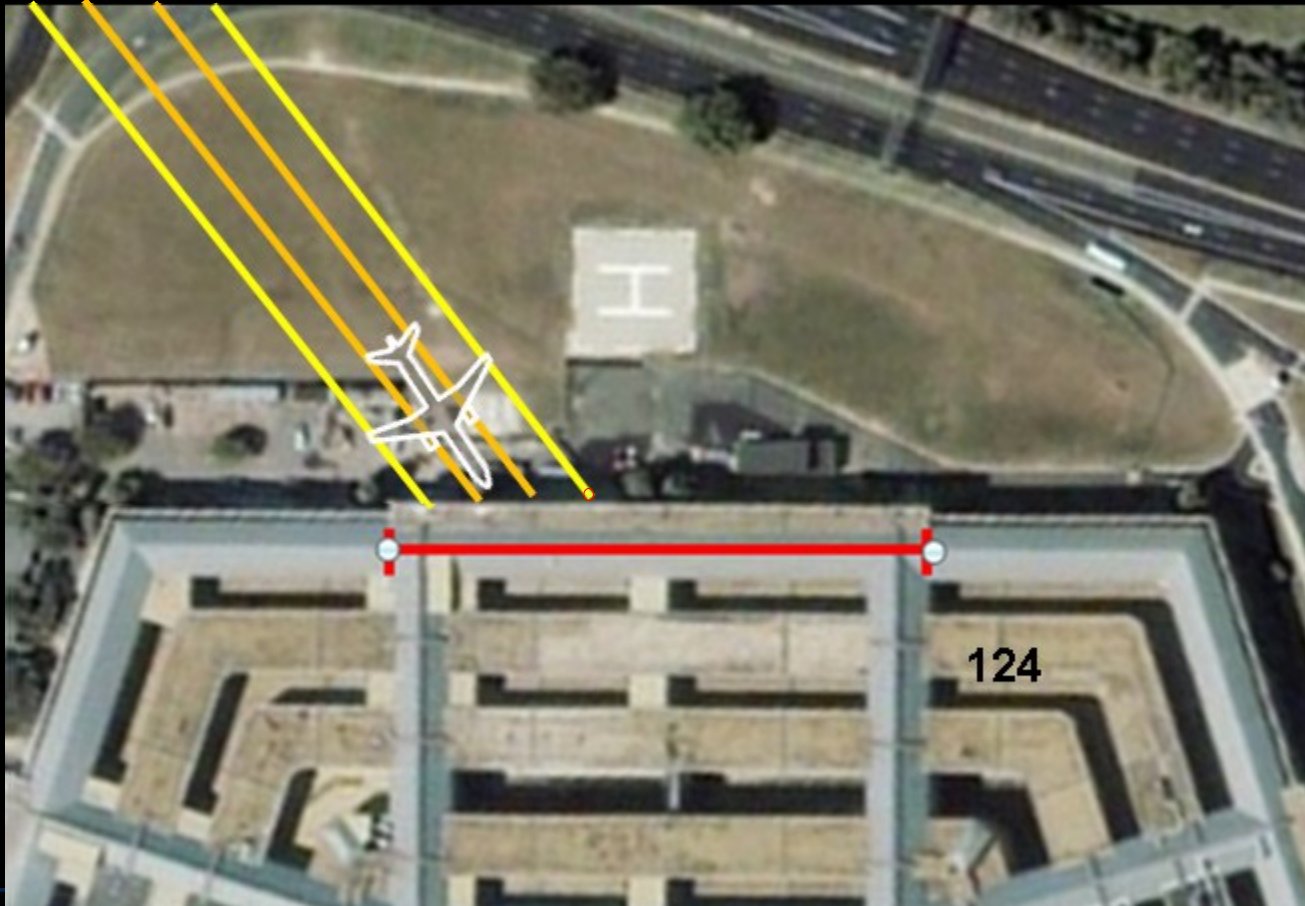
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

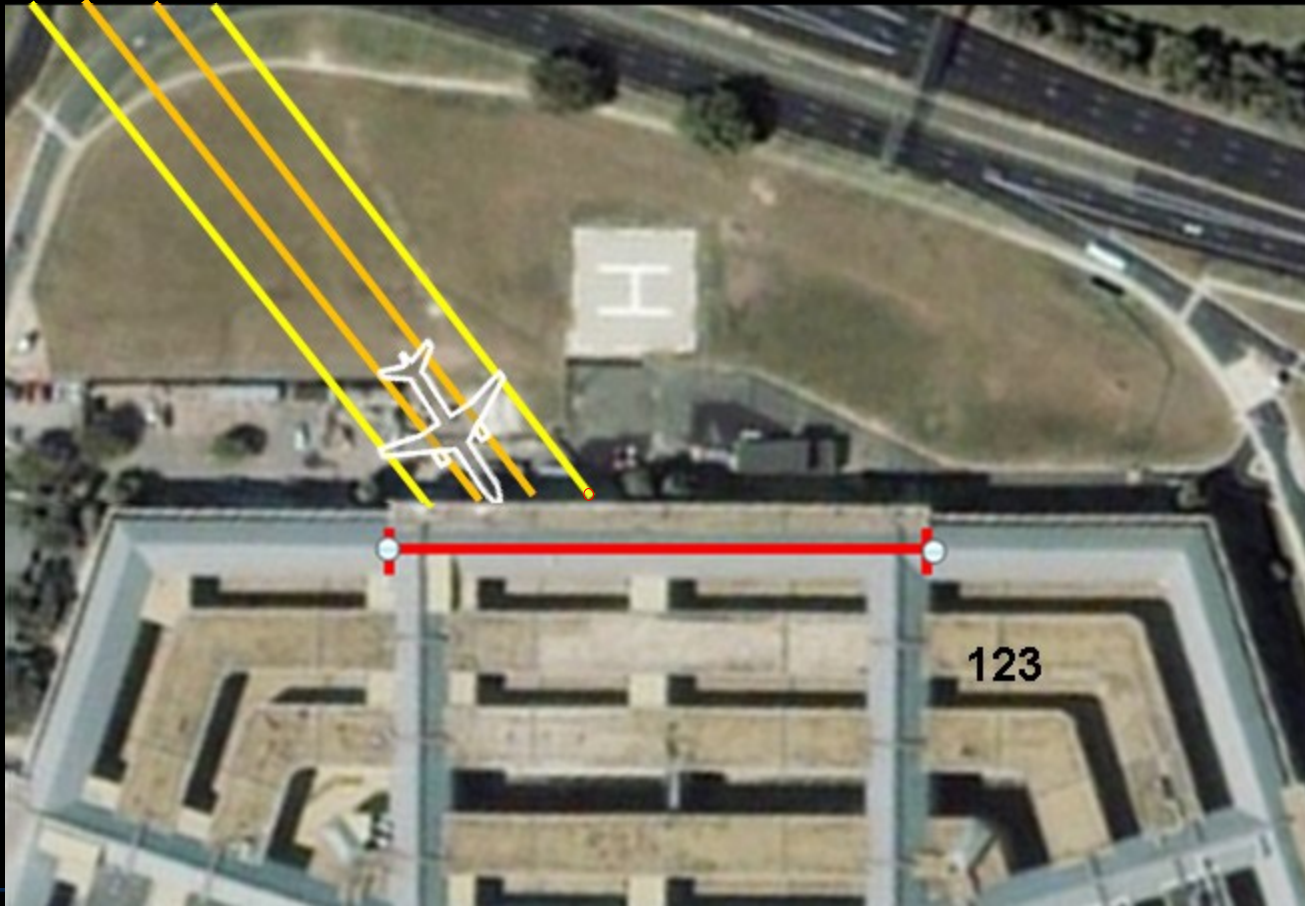
# Continues Toward Façade Probably Accelerating Spools #4 and #7



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

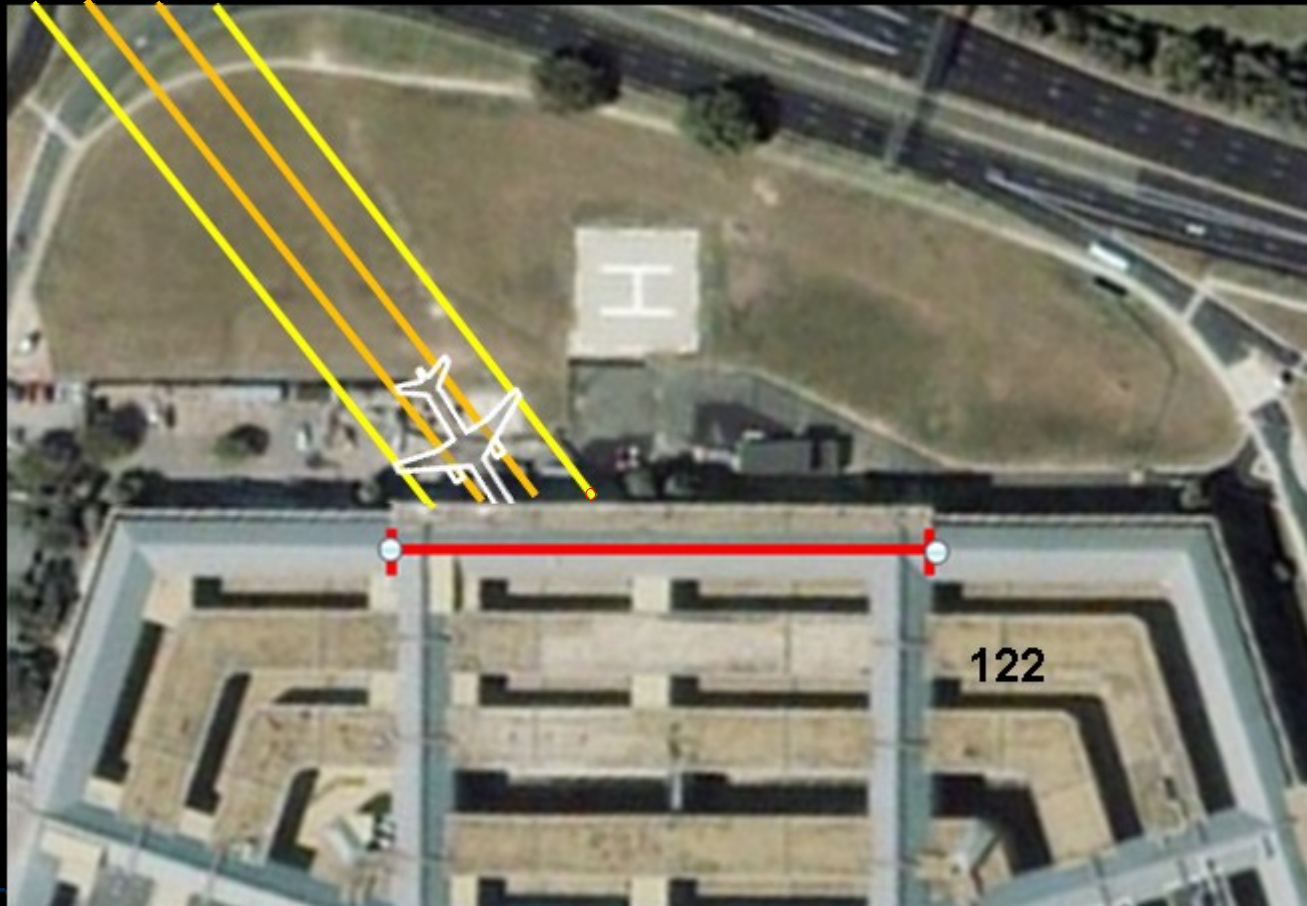
# Continues Toward Façade



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Nose Hits at Column 14



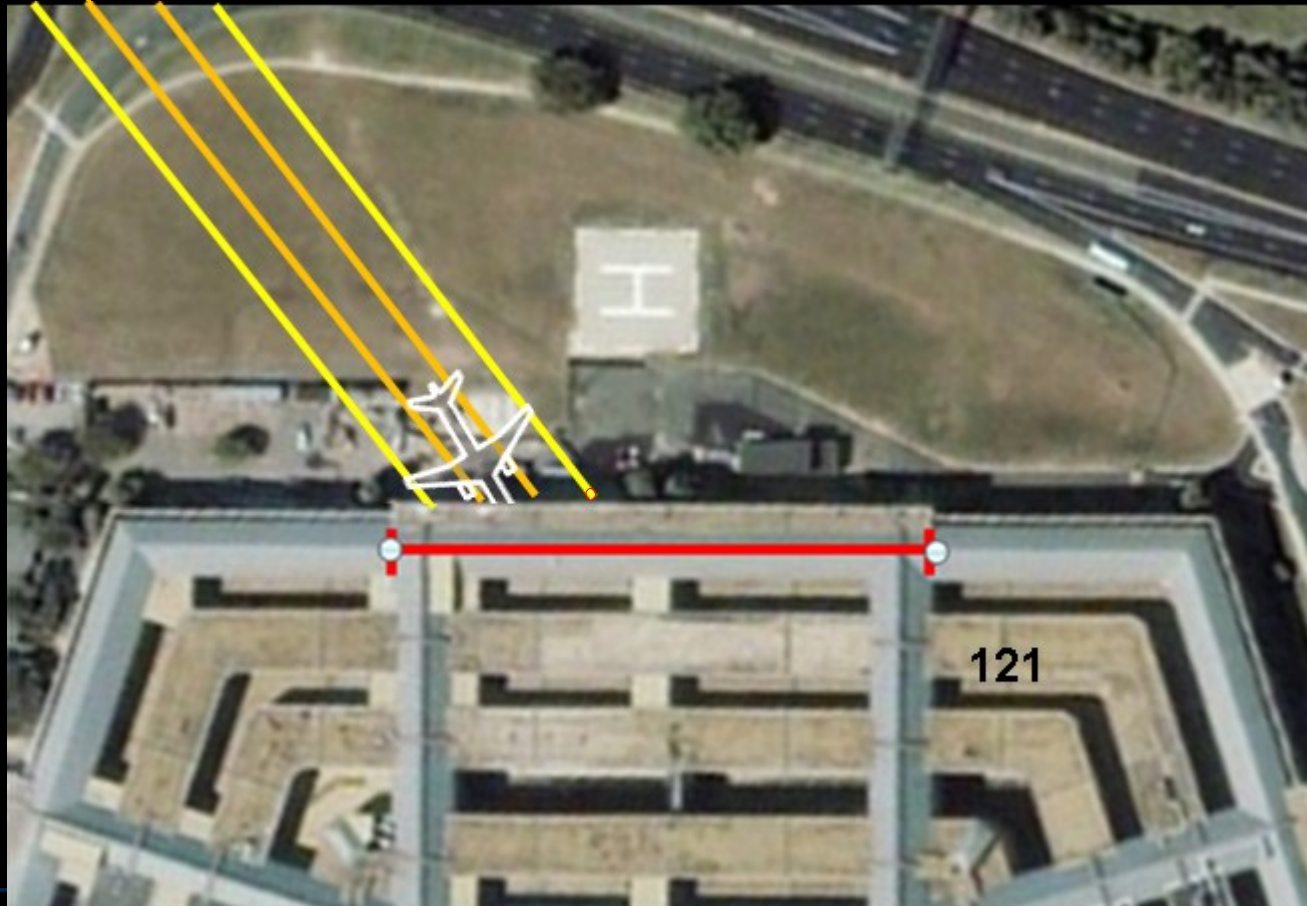
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Continues Impacting Façade

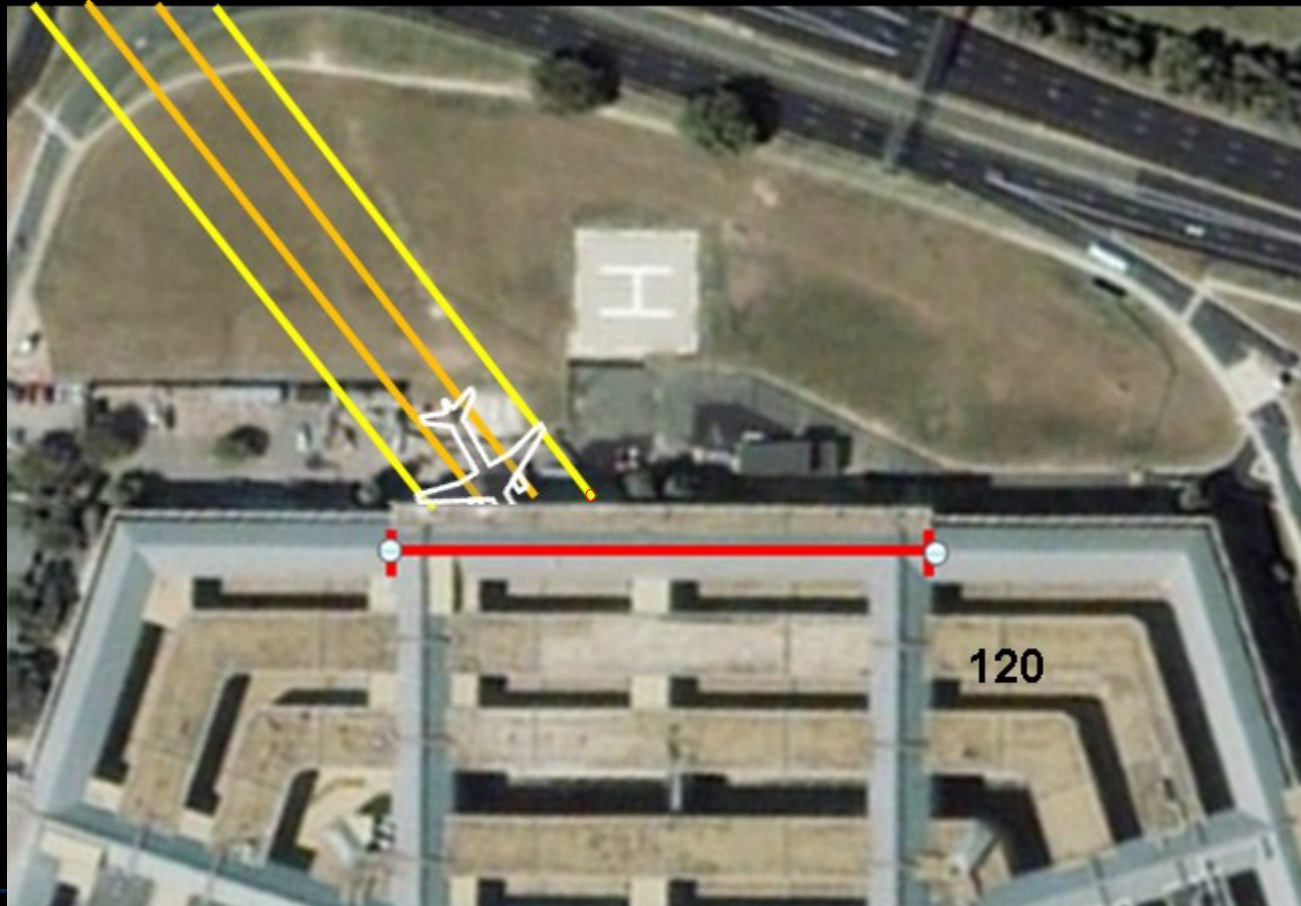


Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017



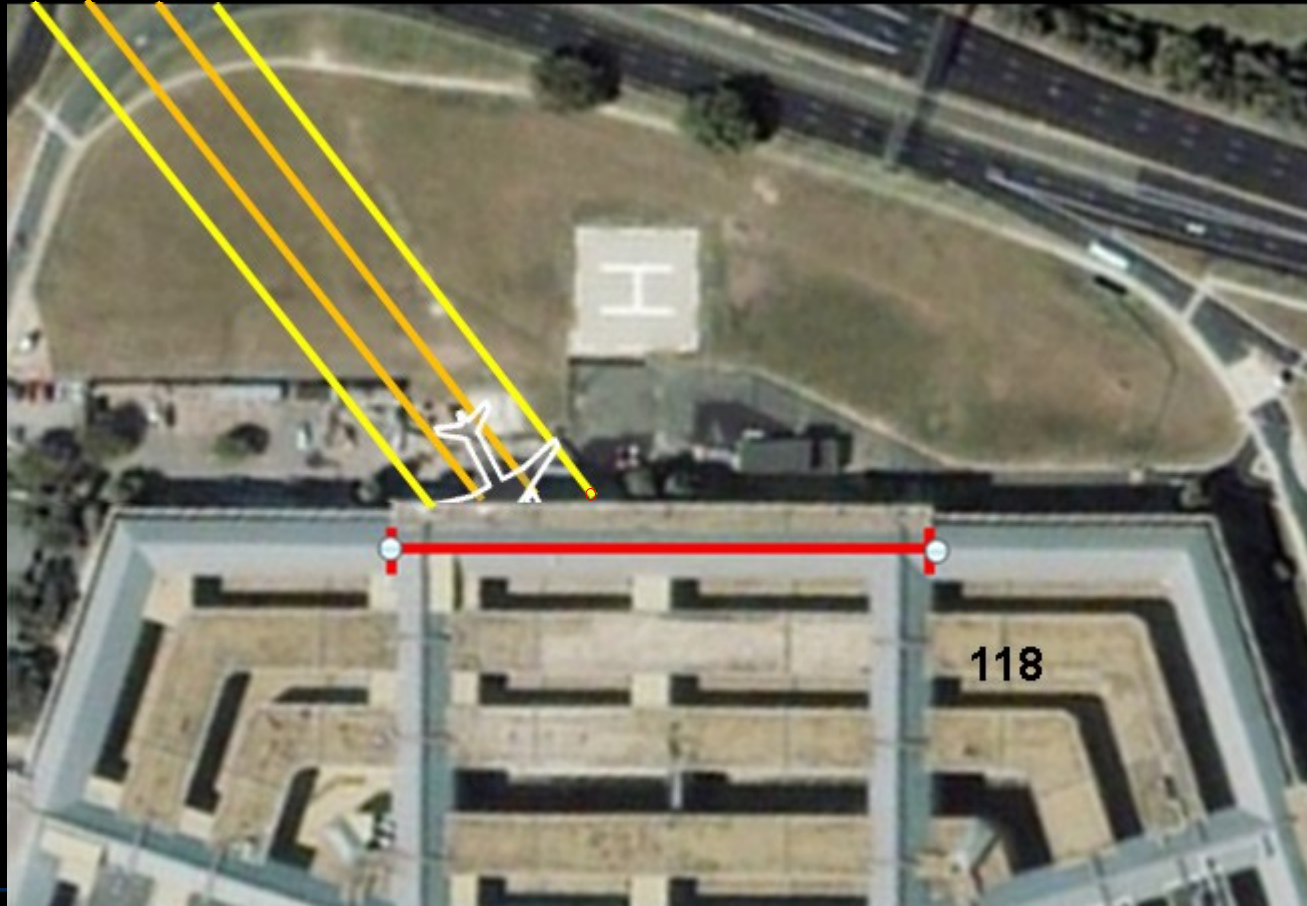
# Right Engine Impacts Window to Left of Column 17



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

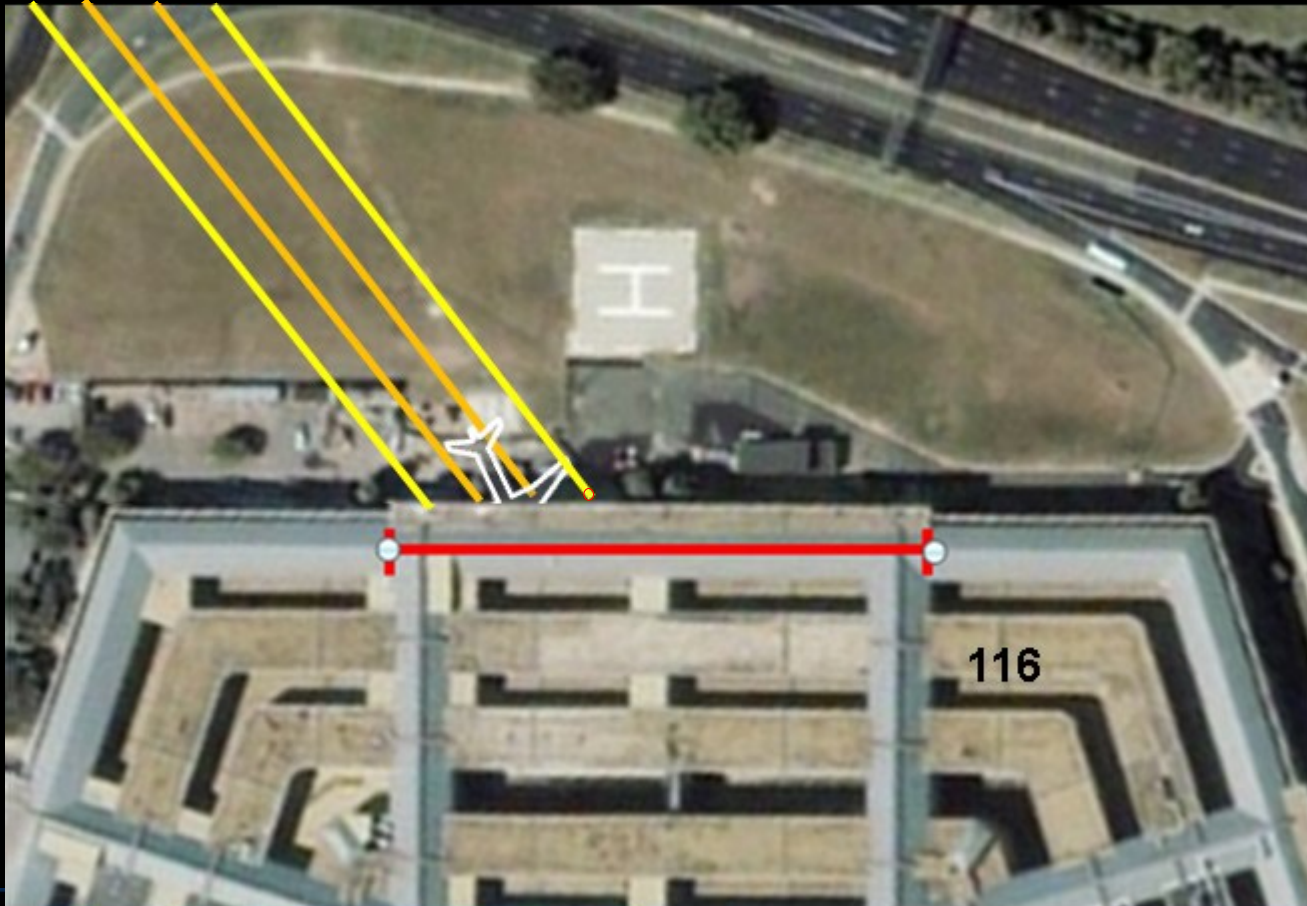
# Right Wing Spar Bashes in Second Floor Columns 18 and 19



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Left Engine Impacts Column 12

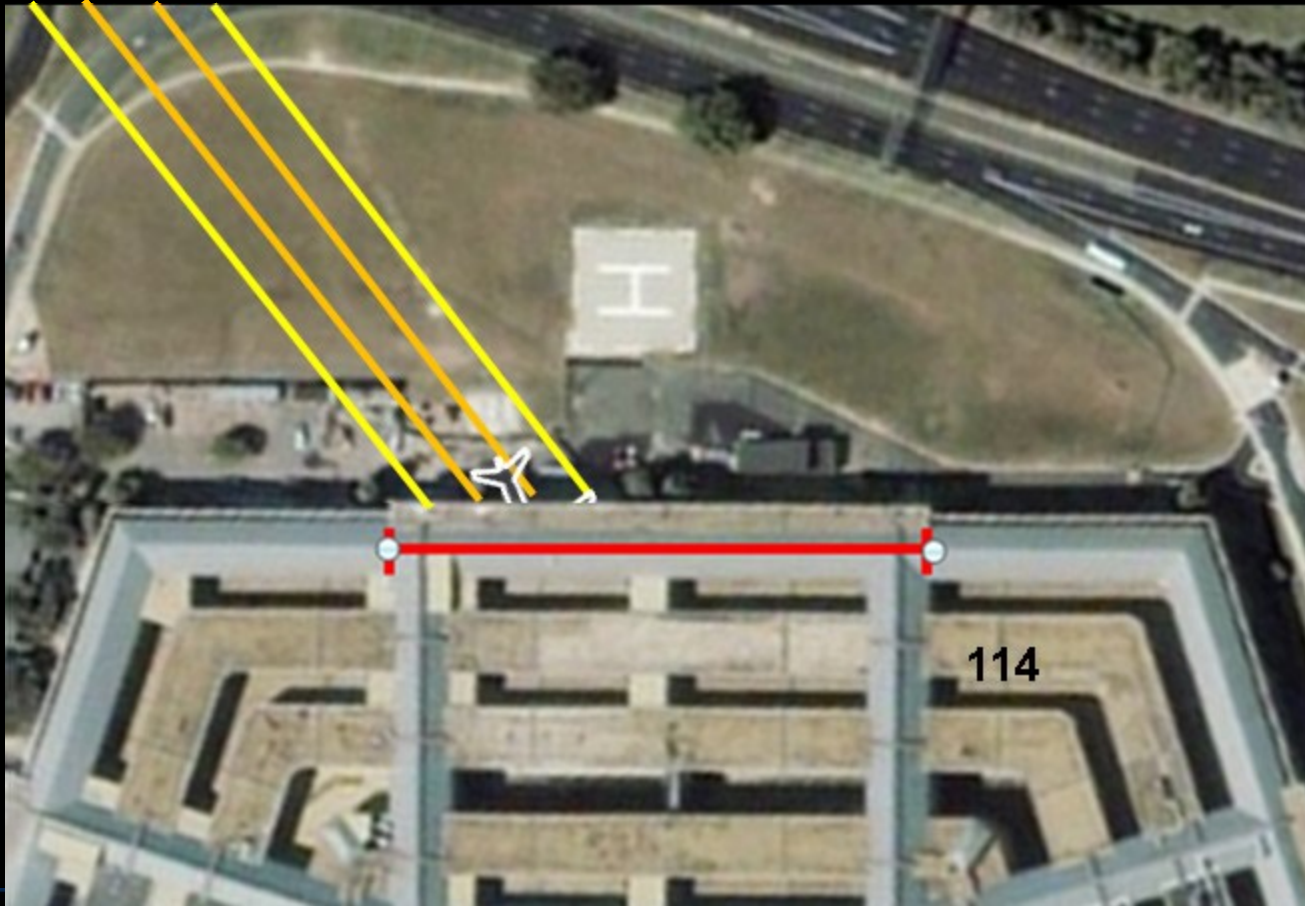


Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Left Wing Impacts Column 9AA

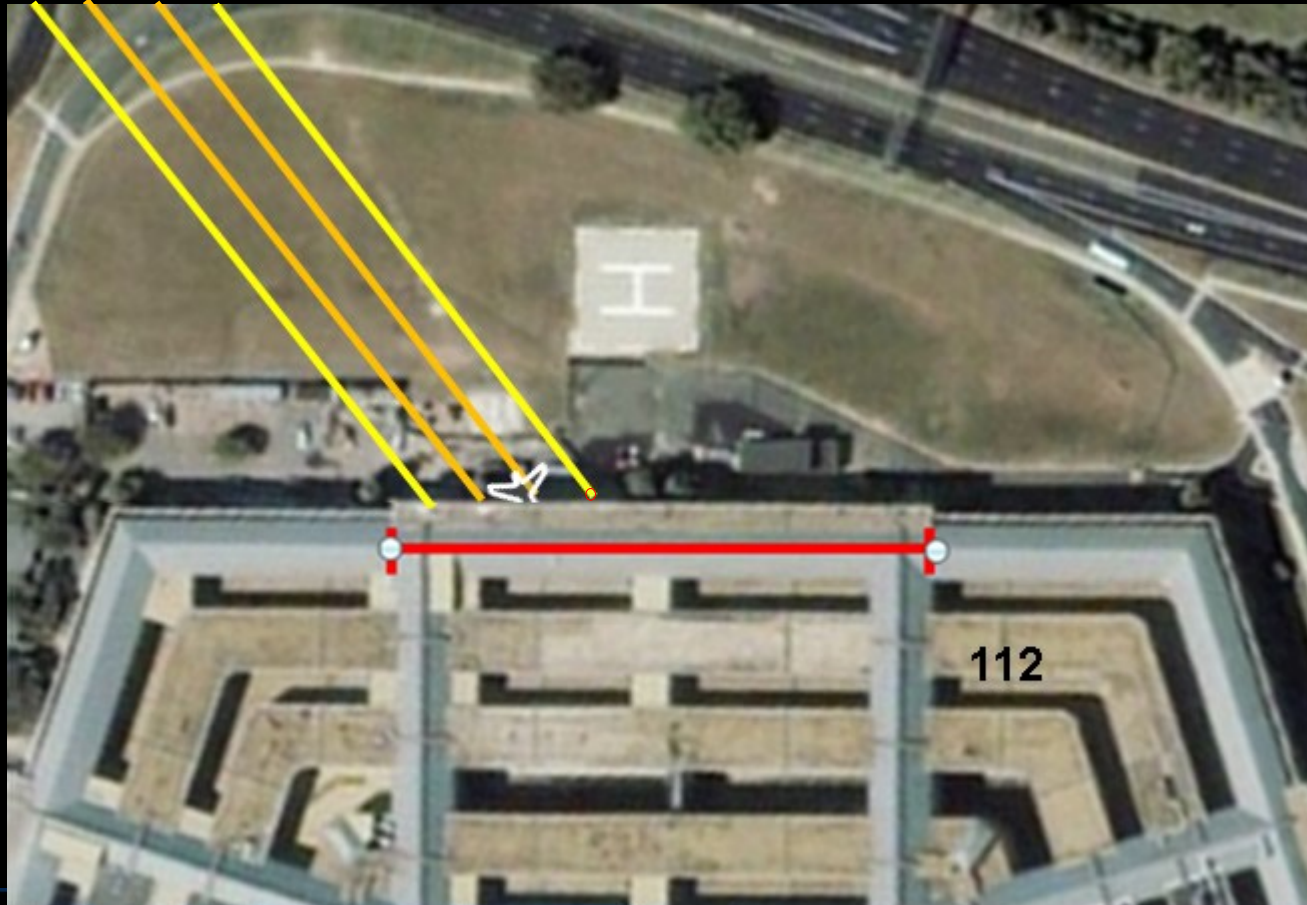
Column 9AA bowed, not severed, because energy in the wing spar dissipated by impact with Columns 10, 11 and 12



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

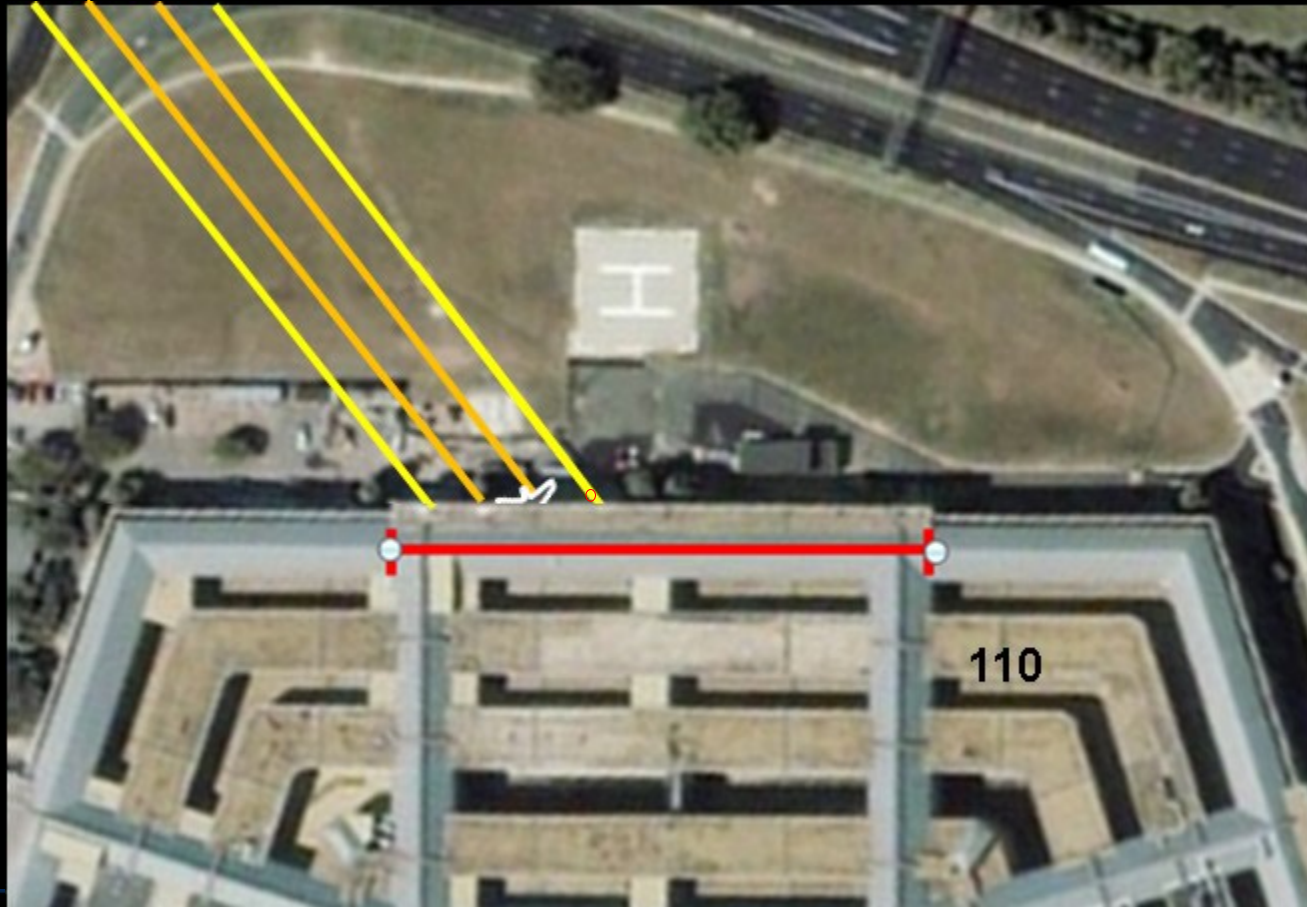
# Bulk of Plane Inside Pentagon



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Tail Impacts Near Column 12



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference 46  
October 25, 2017

Analysis of Pentagon Plane Approach and Impact

# DAMAGE TO KEY LOCATIONS AT THE FAÇADE



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Damage to Columns 18 and 19 on Second Floor

- Evidence of large momentum based impact event
- Consistent with large plane impact
- Cannot be explained by other hypotheses
  - e.g. Exterior explosives
  - e.g. Interior explosives
  - e.g. Cruise missile type event



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State  
Crimes Teleconference  
October 25, 2017



# Columns 18 and 19 Shown “Bashed” Inward



A 501c(3) Non-Profit Educational Organization

news the “free press” isn't  
telling you

9/11 and Other Deep State  
Crimes Teleconference  
October 25, 2017

# Column 19 “Bashed” Inward



Because there is important news the “free press” isn't telling you

**9/11 and Other Deep State Crimes Teleconference**  
**October 25, 2017**

# Column 19 “Bashed” Inward



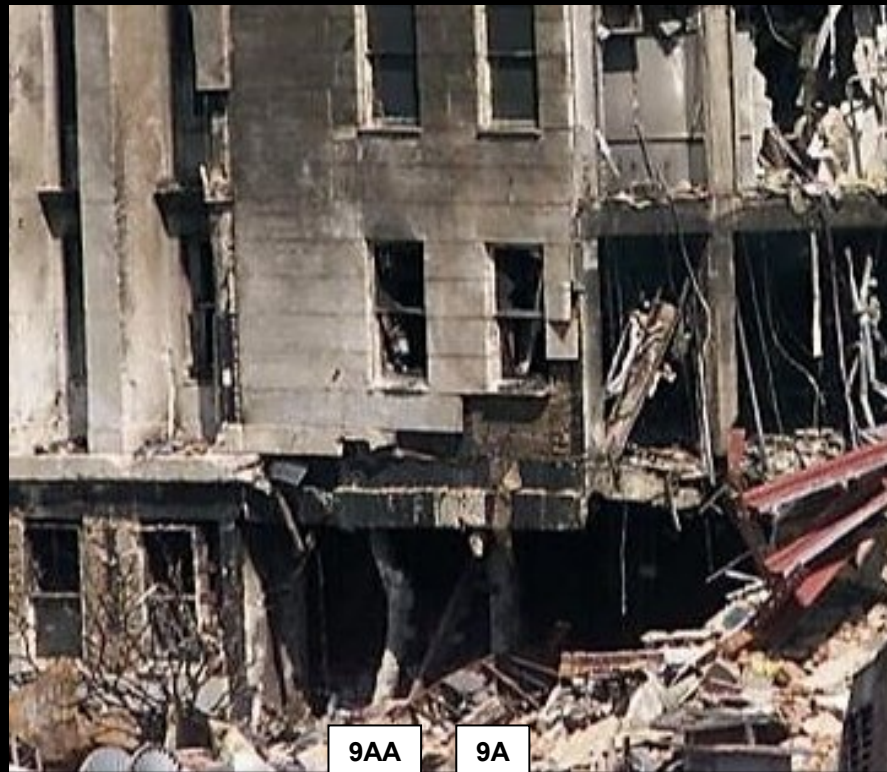
A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn’t telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Column 9AA is Bowed Inward at North End from Wing Impact



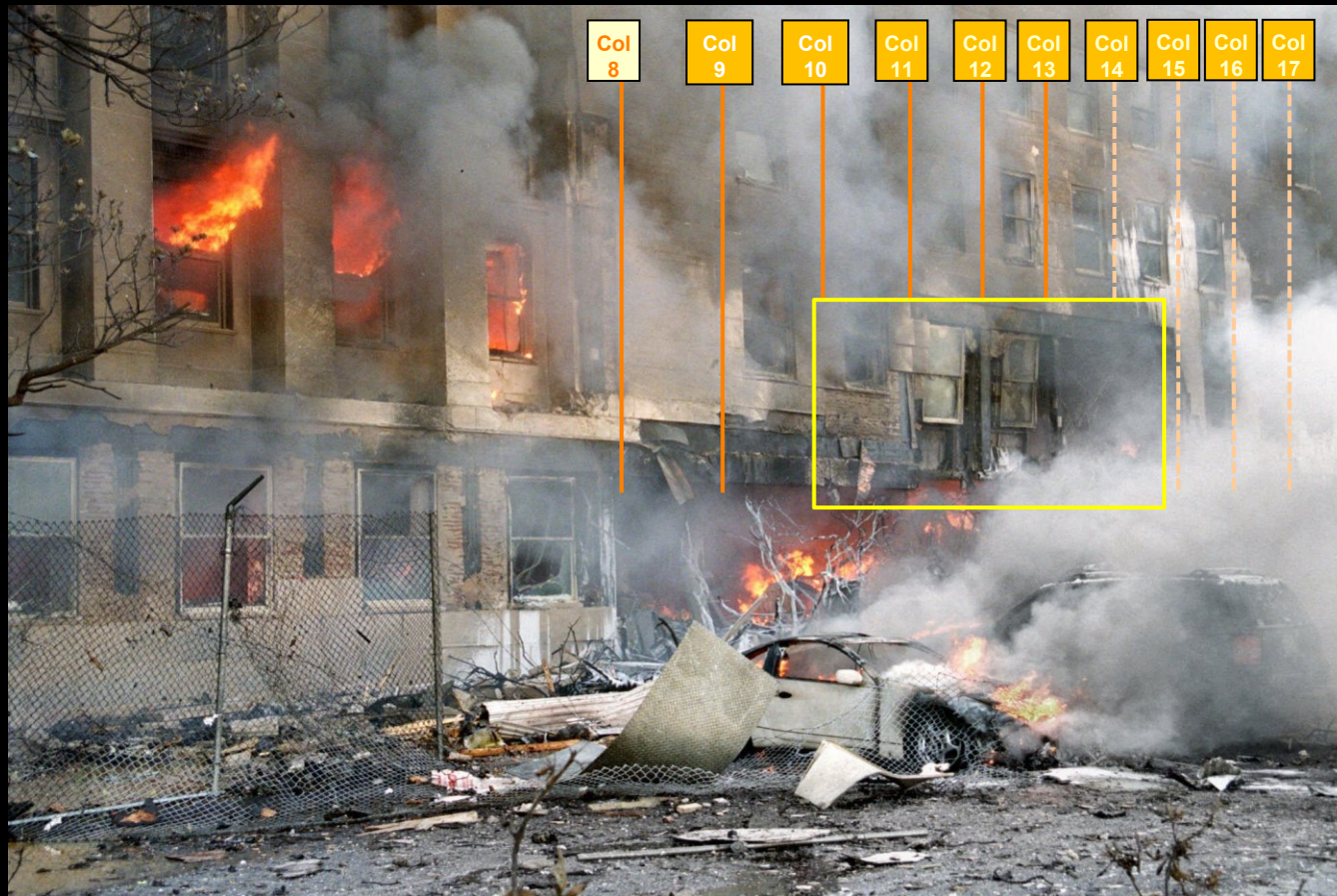
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Damage at Column 12 Suggests Impacting Mass into Second Floor



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Second Floor Beam Missing Under Column 12

Wing and engine impact was significantly below the second floor beam, suggesting another mass damaged this area.



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference 54  
October 25, 2017

Analysis of Pentagon Plane Approach and Impact

# PLANE PROJECTION ON DAMAGE TO FAÇADE



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference 55  
October 25, 2017

# Key Assumptions for Plane Projection

- Impact scaled to 140'
  - Assumes “yaw” (z-axis) rotation
    - Due to right engine firmly impacting generator
    - Right wing impacting the wall
  - Without rotation projection would be 157'
  - Rotation (z-axis orientation) has no significant change direction of momentum
- Both wings flexed 4 ft upward at wingtips
- Impact angle 4 degrees to horizontal



A 501c(3) Non-Profit Educational Organization

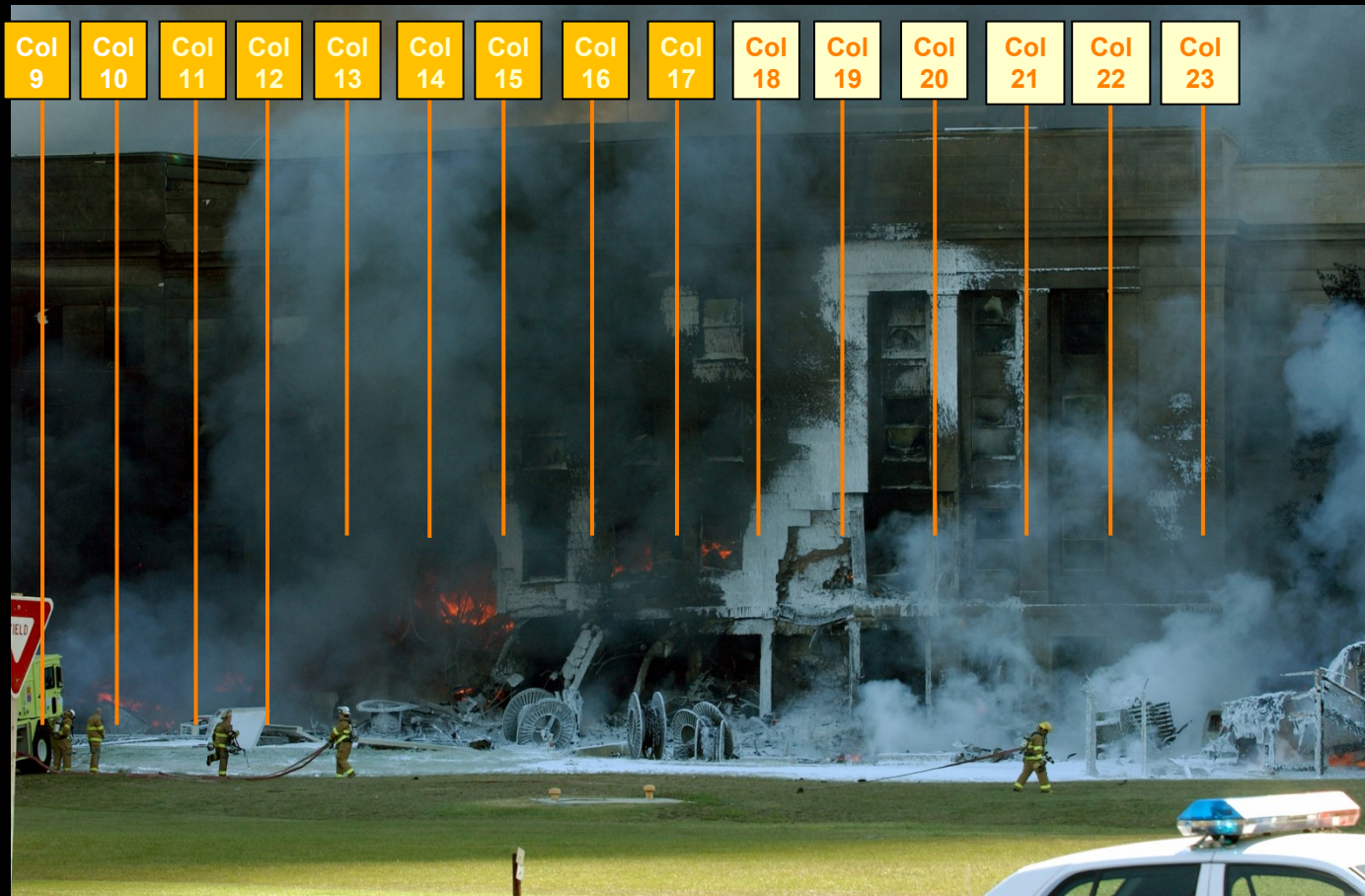


Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017



# West Façade Showing Impact Area



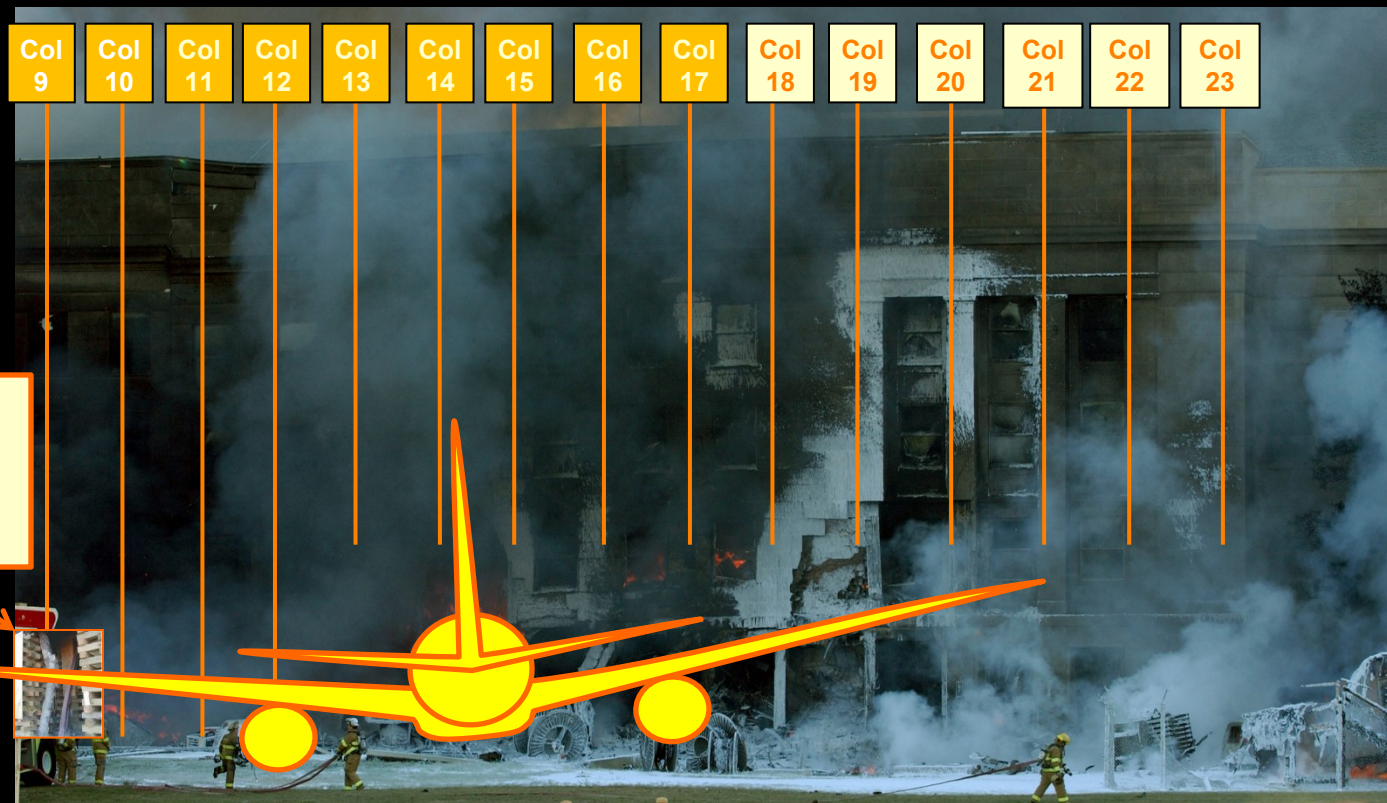
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference 57  
October 25, 2017

# Plane Projection Aligns with Columns 9AA and 19; Tree Stump



Inset Showing Column 9AA

Plane travels from left to right. Due to perspective, items shown in the foreground are not in the plane's travel path. Also, yaw not shown (e.g. tail should be impacting near column 12).



A 501c(3) Non-Profit Educational Organization

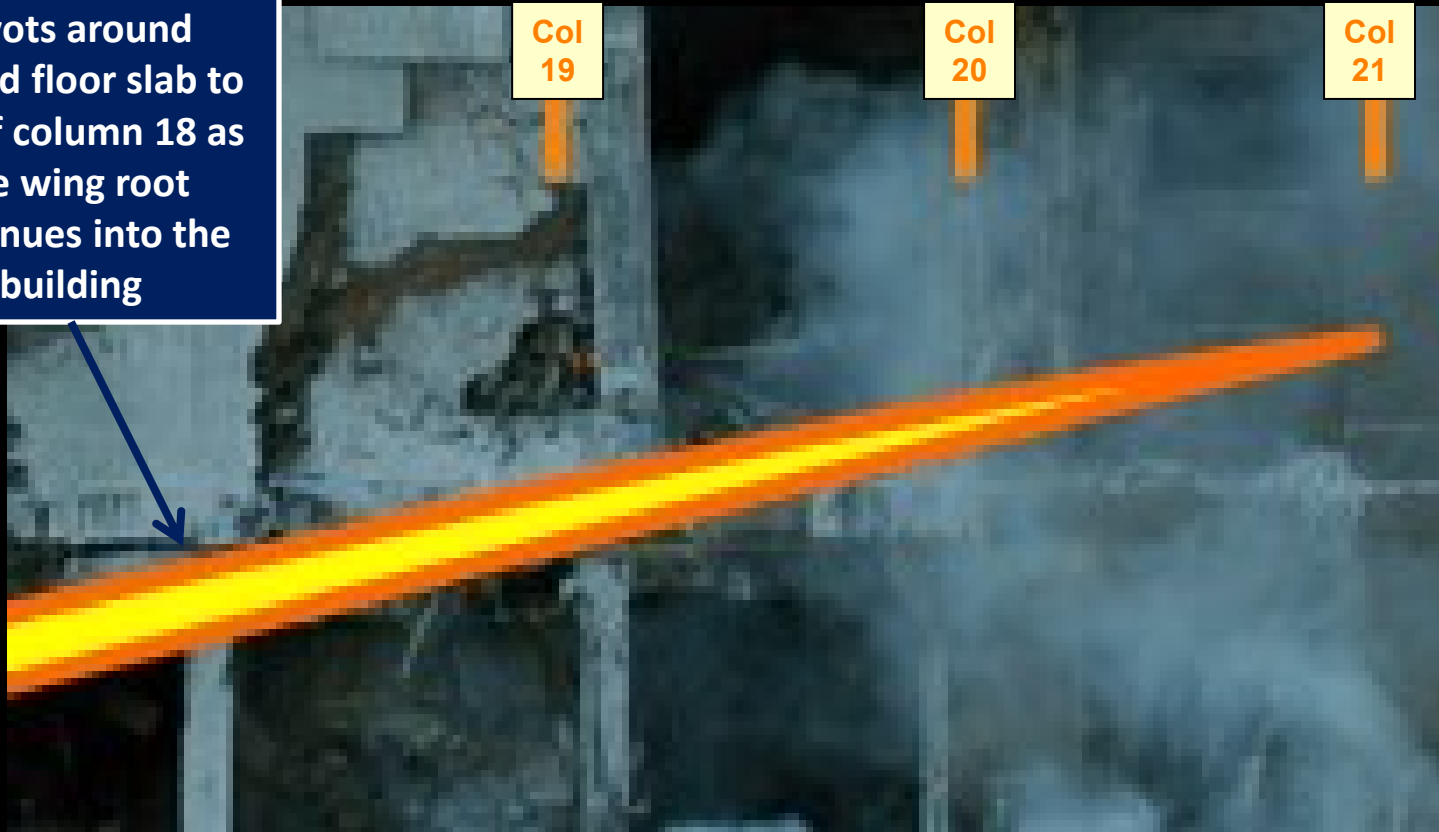


Because there is important news the "free press" isn't telling you

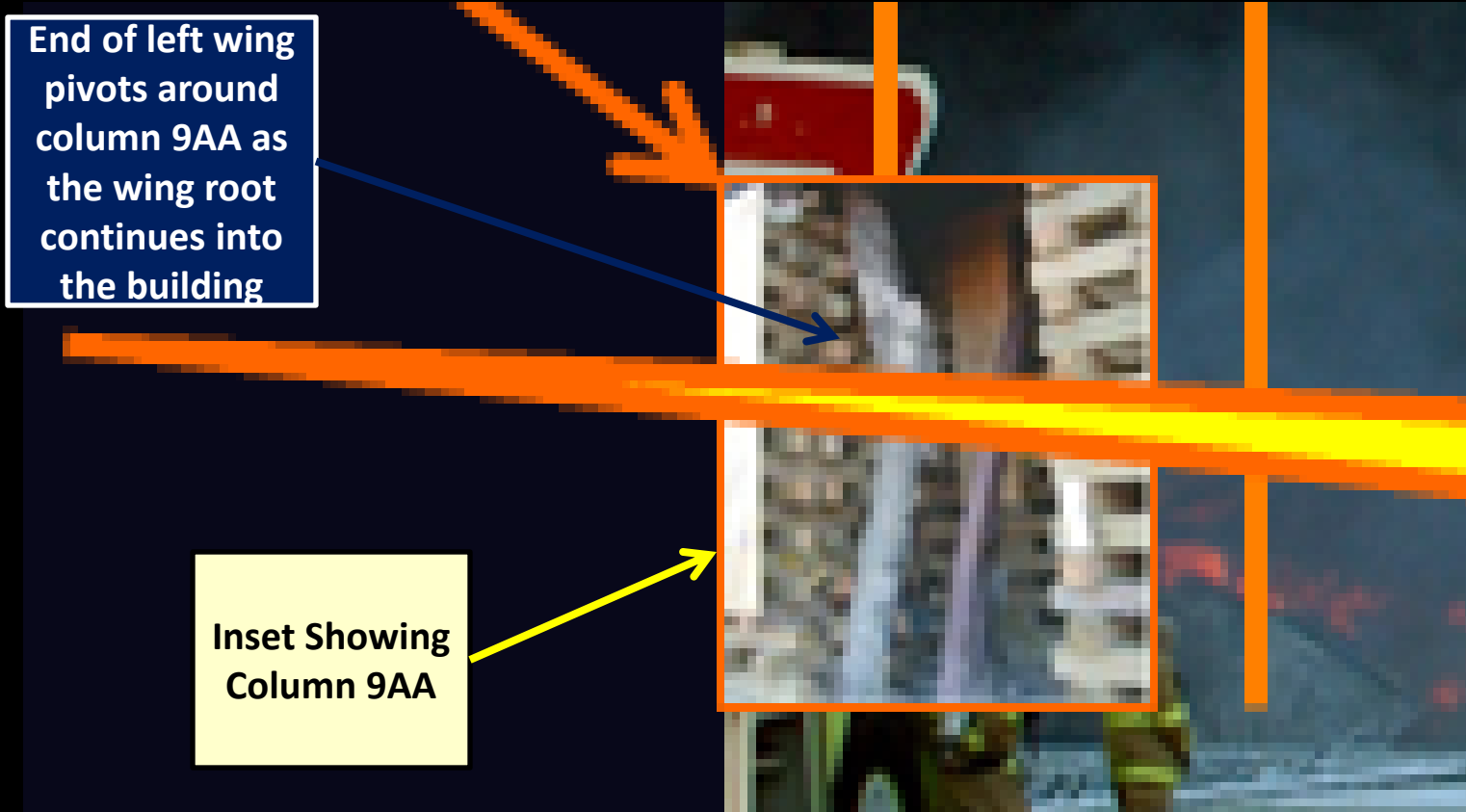
9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Right Wing Impacts and Damages Columns 18 and 19

End of right wing  
pivots around  
second floor slab to  
left of column 18 as  
the wing root  
continues into the  
building



# Left Wing Impacts Column 9AA Leaving Column Bowed Inward



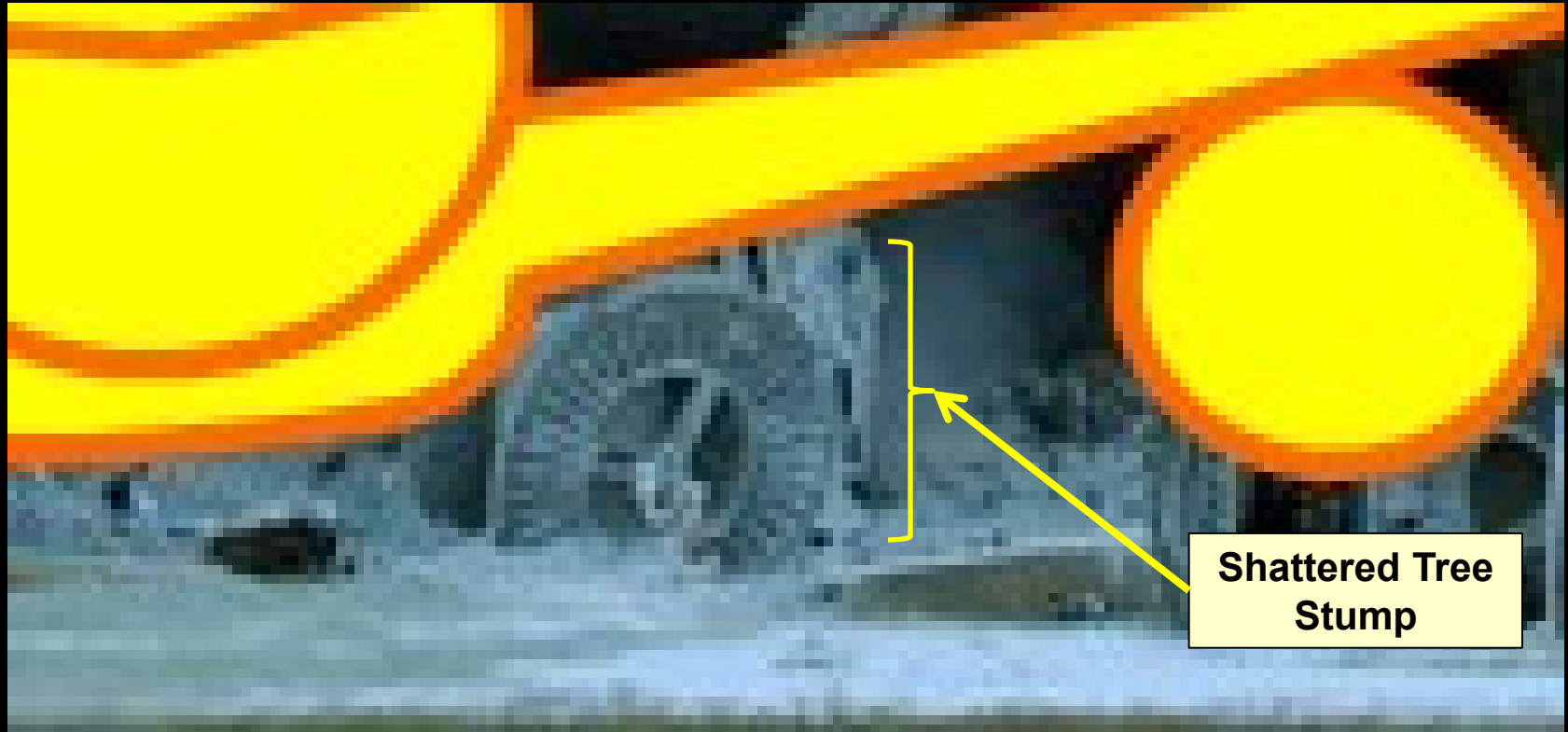
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Projection Shows Tree Stump at Height of Wing Root / Wing Spar



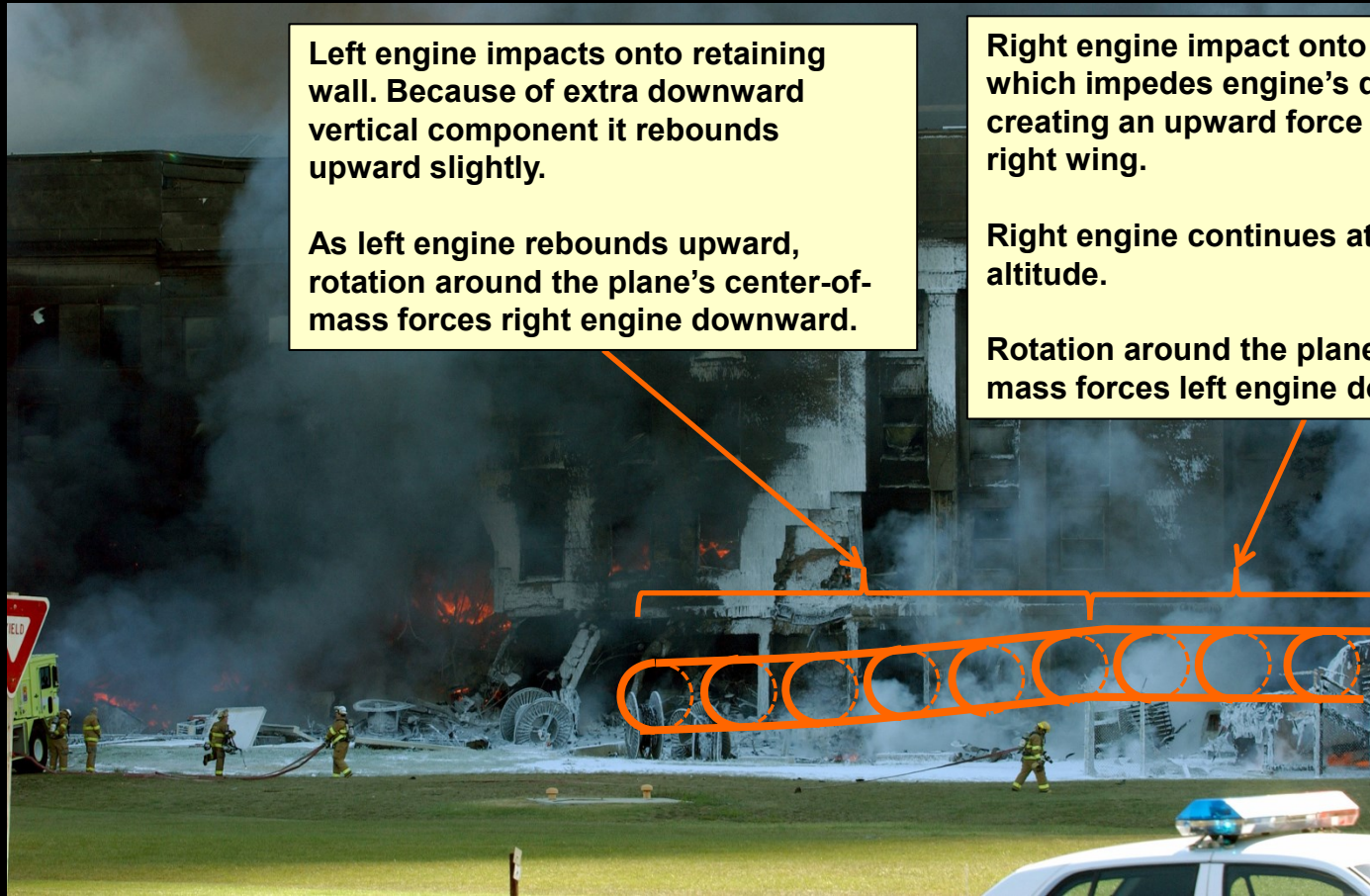
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Trace of Right Engine to the Impact Point on the Façade



Left engine impacts onto retaining wall. Because of extra downward vertical component it rebounds upward slightly.

As left engine rebounds upward, rotation around the plane's center-of-mass forces right engine downward.

Right engine impact onto generator trailer which impedes engine's descent – thus creating an upward force acting upon the right wing.

Right engine continues at about constant altitude.

Rotation around the plane's center-of-mass forces left engine downward.



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

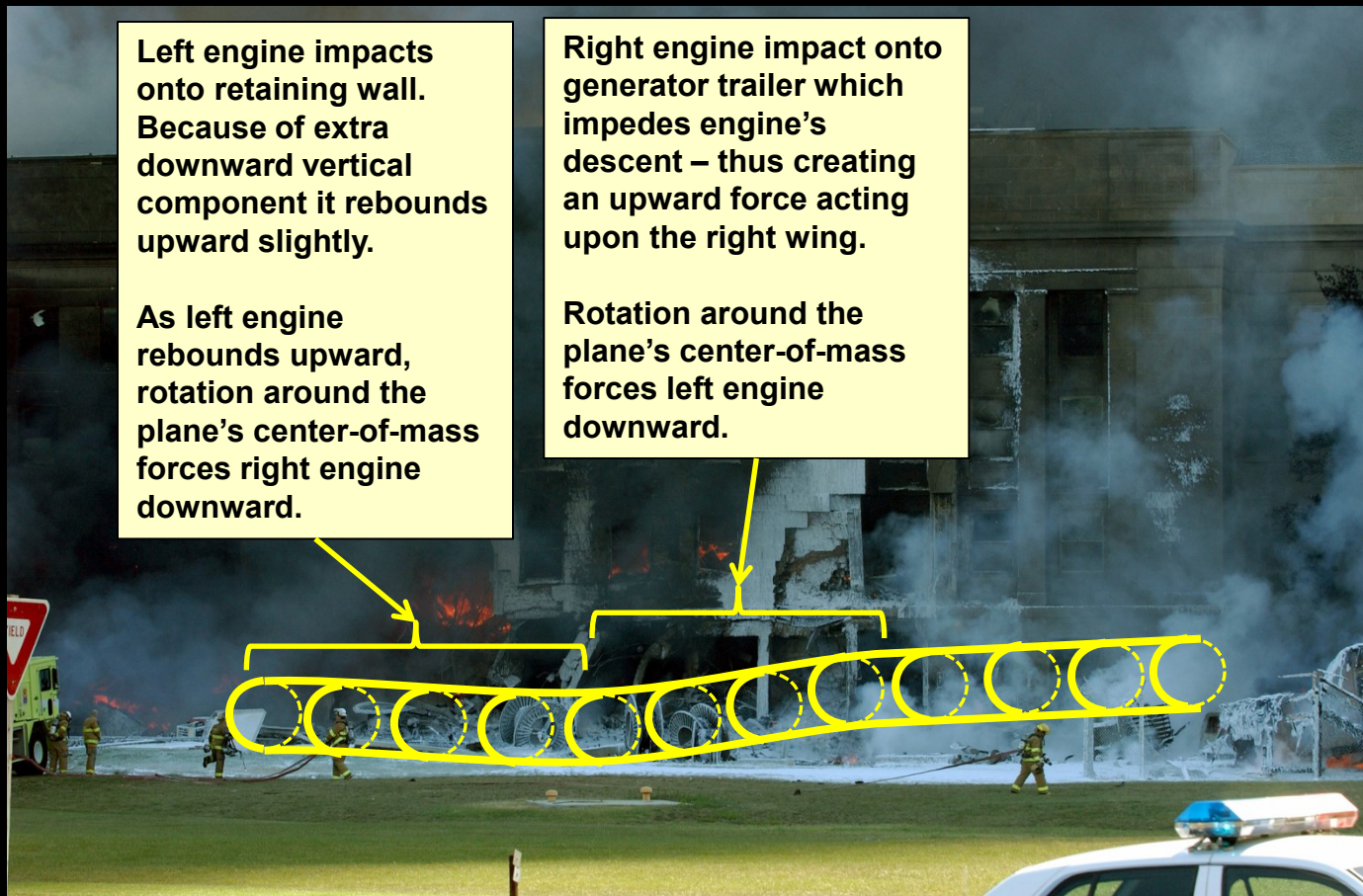
# Trace of Left Engine to the Impact Point on the Façade

Left engine impacts onto retaining wall. Because of extra downward vertical component it rebounds upward slightly.

As left engine rebounds upward, rotation around the plane's center-of-mass forces right engine downward.

Right engine impact onto generator trailer which impedes engine's descent – thus creating an upward force acting upon the right wing.

Rotation around the plane's center-of-mass forces left engine downward.



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Trace of Both Engines to Their Impact onto the Façade



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference 64  
October 25, 2017



Analysis of Pentagon Plane Approach and Impact

# ANSWER TO THE QUESTIONS



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Question After April Presentation About Tree at Column 16

- Question after the April presentation:

*On our call, you said that you thought the tree was severed by the right wing spar.*

*Could you explain how the bottom of the right engine could hit the top of the generator trailer and then the wing dip low enough to sever the tree just a few feet from the ground?*

*And if the wing spar hit the tree, as you suggest, would the engine not gouge the lawn and/or hit some of the spools?*



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State  
Crimes Teleconference  
October 25, 2017

# Answer

- Mass of the plane is descending slightly across the lawn
  - Right engine impacts the generator trailer
    - Right engine cannot descend at same rate as plane
    - Because of rotation around the center-of-mass of the plane, the left engine descends faster
  - Left side engine
    - Impacts the ground at the retaining wall
    - Downward movement arrested and reversed
    - Left wing tip apparently does not hit the ground



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Answer (continued)

- Right side engine
  - The right engine impacts the façade
    - To the left of column 17
    - Near the lower half of the window
    - Traveling horizontally (e.g. no impact into floor slab)
- Left side engine
  - Impacts into Column 12 well below second floor
- Damage to second floor at column 12 suggests tail impact



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference 68  
October 25, 2017

# Answer (continued)

- The wire spools rolled
  - From their storage area between the generator trailer and the retaining wall
    - Rolled in the wake of the plane, or
    - Moved because of impact with the plane
  - They came to their final post-impact location after the plane had passed
- There is no photographic record showing two of the seven wire spools
  - Suggests their destruction upon impact, or
  - Rolling into the Pentagon opening in first floor



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Answer (continued)

- Aligning the key damage points positions the plane
  - Second floor wing impact at column 19 at about one foot above floor slab, places the right wing
  - First floor column 9AA bowed inward places the left wing location
  - Severed tree stump in front of column 16 establishes height of wing root near fuselage



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Questions



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

**9/11 and Other Deep State Crimes Teleconference** 71  
**October 25, 2017**

Analysis of Pentagon Plane Approach and Impact

# OTHER PHOTOS SHOWING SPOOL LOCATION



A 501c(3) Non-Profit Educational Organization

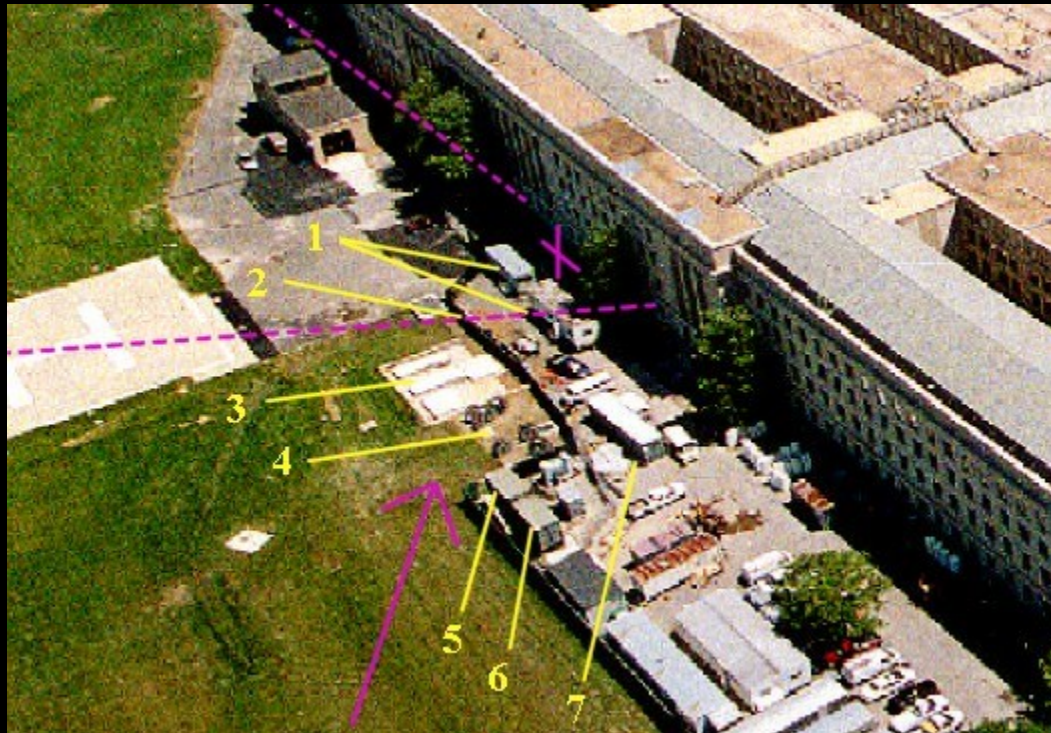


Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017



# Photo with Spools Visible



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference 73  
October 25, 2017

# Photo with Spools Visible



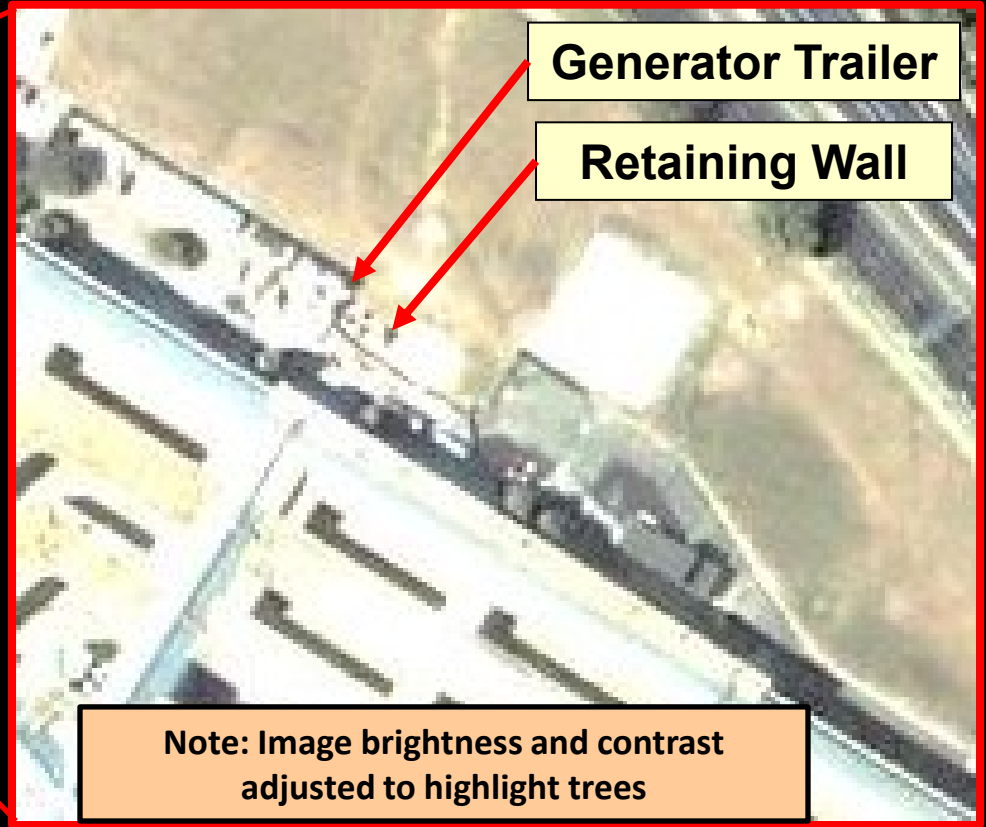
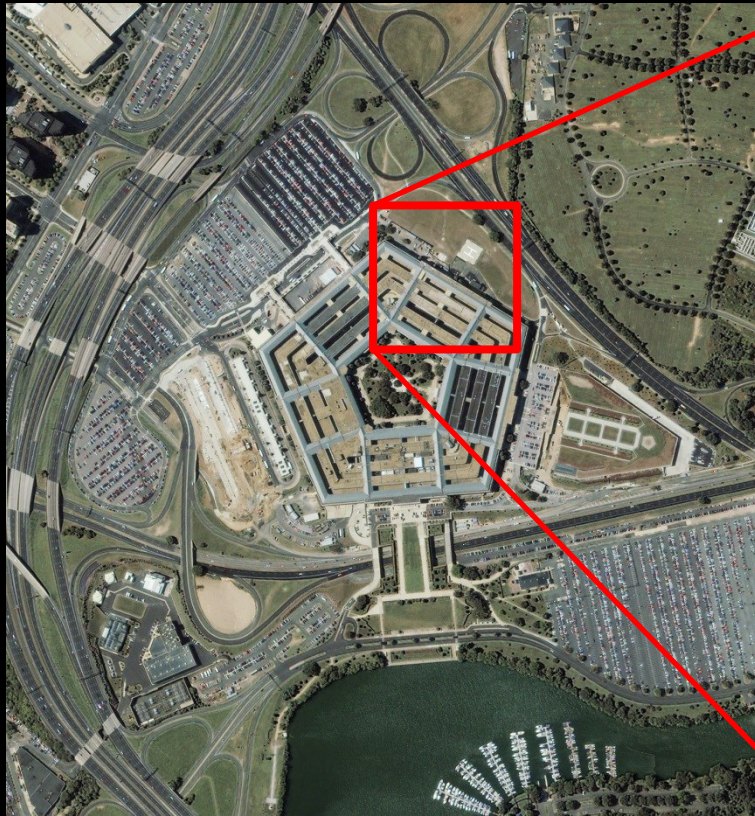
A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference 74  
October 25, 2017

# Satellite Photograph on Sept 7



Source: [https://web.archive.org/web/20050212002810/http://www.spaceimaging.com/gallery/9-11/Pentagon/Pentagon\\_9\\_7\\_01.jpg](https://web.archive.org/web/20050212002810/http://www.spaceimaging.com/gallery/9-11/Pentagon/Pentagon_9_7_01.jpg)



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference 75  
October 25, 2017

Analysis of Pentagon Plane Approach and Impact

# APPENDIX “PILOTS FOR 9/11 TRUTH” G-FORCE CALCULATION



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Overview

- Pilots for 9/11 Truth use a simple equation for centripetal force to estimate G-Forces
  - Applied to aircraft descending from
    - Above the VDOT tower to
    - Become level as passing street lamp pole #1
    - Remain level to the West Façade of the Pentagon
  - Pilots for 9/11 Truth calculate 10.14 G-Force
    - G-Forces too high for a Boeing 757 aircraft
    - Purports to rule-out a large plane impact



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# G-Force Calculation

- Based on their calculated G-Force
  - Plane pulling up out of a dive
  - Assume plane travels in arc with a 2,085' radius
- Pilots for 9/11 Truth make the following conclusion:

[at 9:50] ... for the least challenging "pull." If we hypothetically lower the aircraft altitude from the NTSB plotted altitude, to the lower height of the VDOT antenna. As we can see, the G-loads to "pull" out of a dive from the top of the VDOT antenna are impossible for a 757.

Source: <https://www.youtube.com/watch?v=PtlzCyKbw5Q>



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Formula for Centripetal Force

G FORCES - Scene From 9/11: ATTACK ON THE PENTAGON

Home - Ask Questions - L  
Site Map - Free

## MULAS

tion Bar

on Menu

Motion

3D Motion

of Motion

Energy Power

lar Motion

avitation

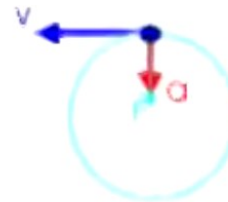
es of Matter

ectricity

olve problems

EXAMPLES

## Circular motion



In the diagram **v** is the tangential velocity of the object. **a** is the centripetal (acting towards the center of the circle) acceleration and **F** is the centripetal force. **r** is the radius of the circle and **m** is mass of the object.

$$a = v^2 / r$$

$$F = ma = mv^2/r$$

## Center of mass

8:52 / 13:00

Source: <https://www.youtube.com/watch?v=PtlzCyKbw5Q>



A 501c(3) Non-Profit Educational Organization

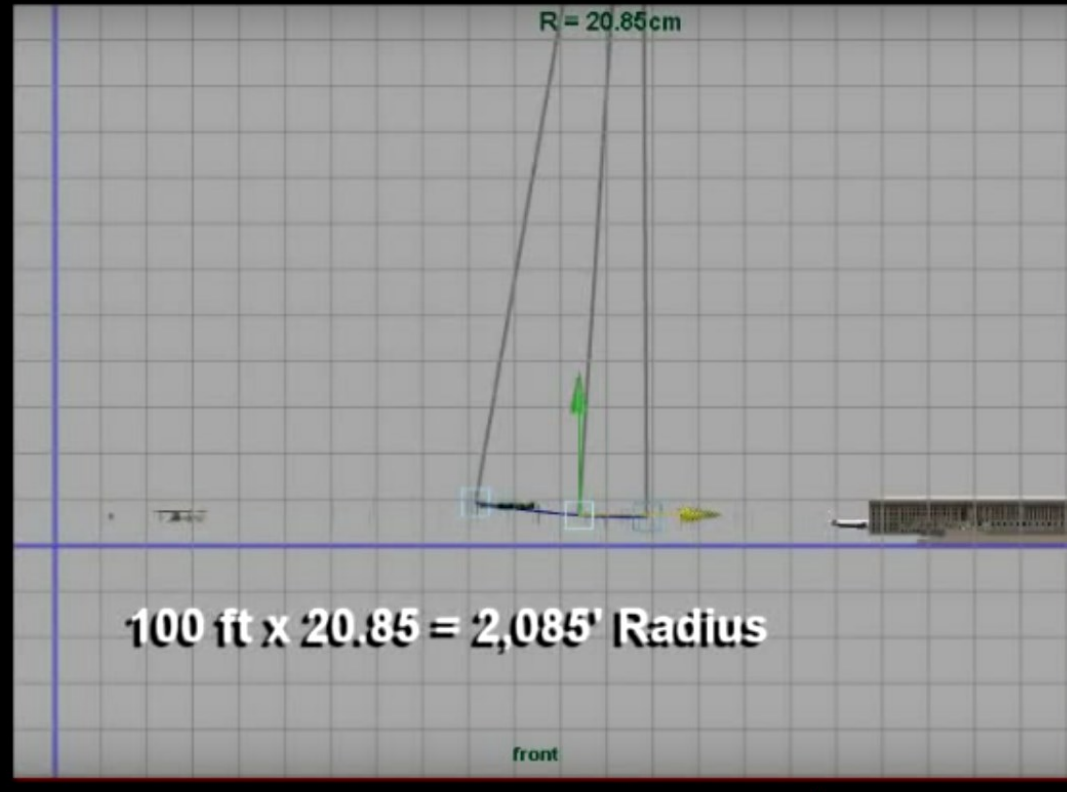


Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Pilot's For 9/11 Truth Problem Set-Up with 2,085 Foot Radius

G FORCES - Scene From 9/11: ATTACK ON THE PENTAGON



▶ ▶ 🔊 8:41 / 13:00

Source: <https://www.youtube.com/watch?v=PtlzCyKbw5Q>

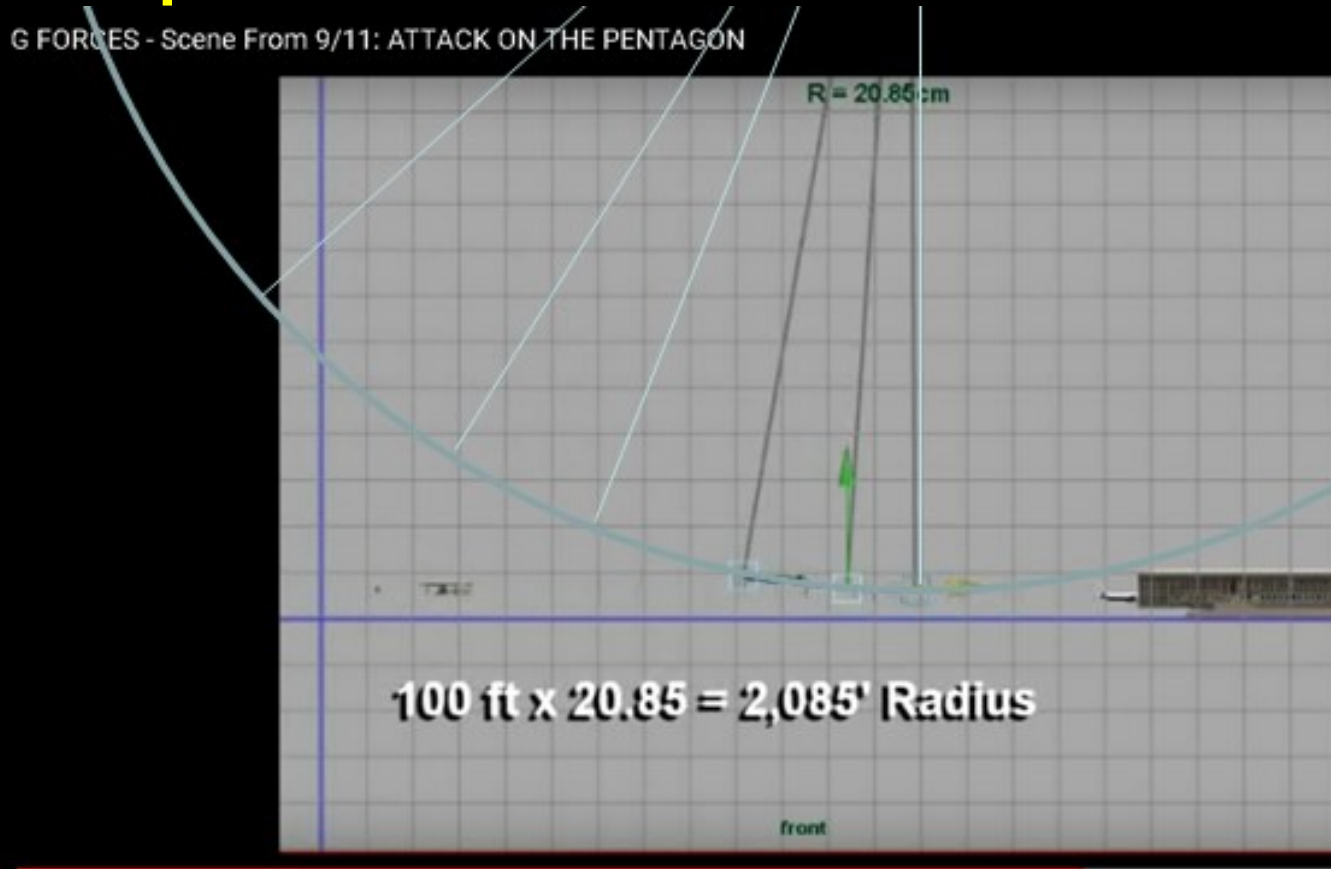


Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference 80  
October 25, 2017



# Pilot's For 9/11 Truth Problem Set-Up with 2,085 Foot Radius



▶ ▶ 🔊 8:41 / 13:00



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Complete 2,085 foot Radius Arc Shown

G FORCES - Scene From 9/11: ATTACK ON THE PENTAGON

R = 20.85m

100 ft x 20.85 = 2,085' Radius

front

▶ ⏪ 🔊 8:41 / 13:00

**Note:**  
Extreme UPWARDS G-Forces required to enter the 2,085 foot radius arc.

Source: <https://www.youtube.com/watch?v=PtlzCyKbw5Q>



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Calculations Supporting 10.14 G-Force

G FORCES - Scene From 9/11: ATTACK ON THE PENTAGON

$$V = 781 \text{ f/s}^2 = 609961$$

$$609961/2085 = 292.2 \text{ f/s}^2$$

$$\text{G Force} = (292.2 \text{ f/s}^2)/32 \text{ f/s}^2 =$$

$$\text{G Force} = 9.14 \text{ G}$$

$$\text{Total G Force Required} = 10.14 \text{ G}$$

▶ ▶ 🔊 9:32 / 13:00

Source: <https://www.youtube.com/watch?v=PtlzCyKbw5Q>



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Corrections Required

- Pilot's key assumption is unsupportable
  - No justification given for the 2,085 foot radius assumption
  - Radius, drawn to scale on the topography, shows impossibility of the assumption
- 16,300 foot radius fits the observation
  - Estimates force of approximately 1.16 G-Force
  - Easily within design limits of a Boeing 757



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# G-Force Conclusion

- Radius of:
  - 16,300' consistent with approach path from VDOT tower
    - G-Force of ~1.16 is consistent with
    - Within design limits of a Boeing 757
  - 2,085' **not consistent** with approach path from VDOT tower
- Pilots for 9/11 Truth G-Force analysis is unsupported and should be corrected



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

Analysis of Pentagon Plane Approach and Impact

# APPENDIX: POST PRESENTATION QUESTIONS



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Post Presentation Question Internal Damage Pattern

- A comment was made regarding the approach path and damage at the C-Ring:

*It doesn't match the official path and it doesn't match the path that you have shown us in past presentations. It also doesn't line up with the damage including the C-ring hole.*

- Reply: Slide 18 now shows the projection to the C-Ring exit hole and internal damage path is depicted correctly



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State  
Crimes Teleconference  
October 25, 2017

# Post Presentation Question Regarding the G-Force Calculation

- Another comment was made regarding the topography and implications for the

*It's very clear that Wayne's blatantly false path allows him to claim the plane flew below the top of the antenna. Based on this, he argues that the G forces would be well within the capabilities of a 757. It is anything but "quibbling" to point this out. Wayne is attempting yet again to undermine a key piece of evidence the Truth Movement has to show the official story to be false.*



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State  
Crimes Teleconference  
October 25, 2017



# Pilots for 9/11 Truth Calculate Upward G-Forces of 10.14G

To get their upward 10.14 G-Force, they assume the 2,085 radius arc for the approaching plane travel.

The plane must travel along this arc for their calculation to be valid.

~~But the plane cannot transition into this 2,085 radius arc from the path from Sheraton or VDOT.~~



620 ft radius gives: 30 G-Force



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Pilots for 9/11 Truth Calculate Upward G-Forces of 10.14G

To get their upward 10.14 G-Force, they assume the 2,085 radius arc for the approaching plane travel.

The plane must travel along this arc for their calculation to be valid.

**But the plane cannot transition into this 2,085 radius arc from the path from Sheraton or VDOT.**



452 ft radius gives: 42 G-Force



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

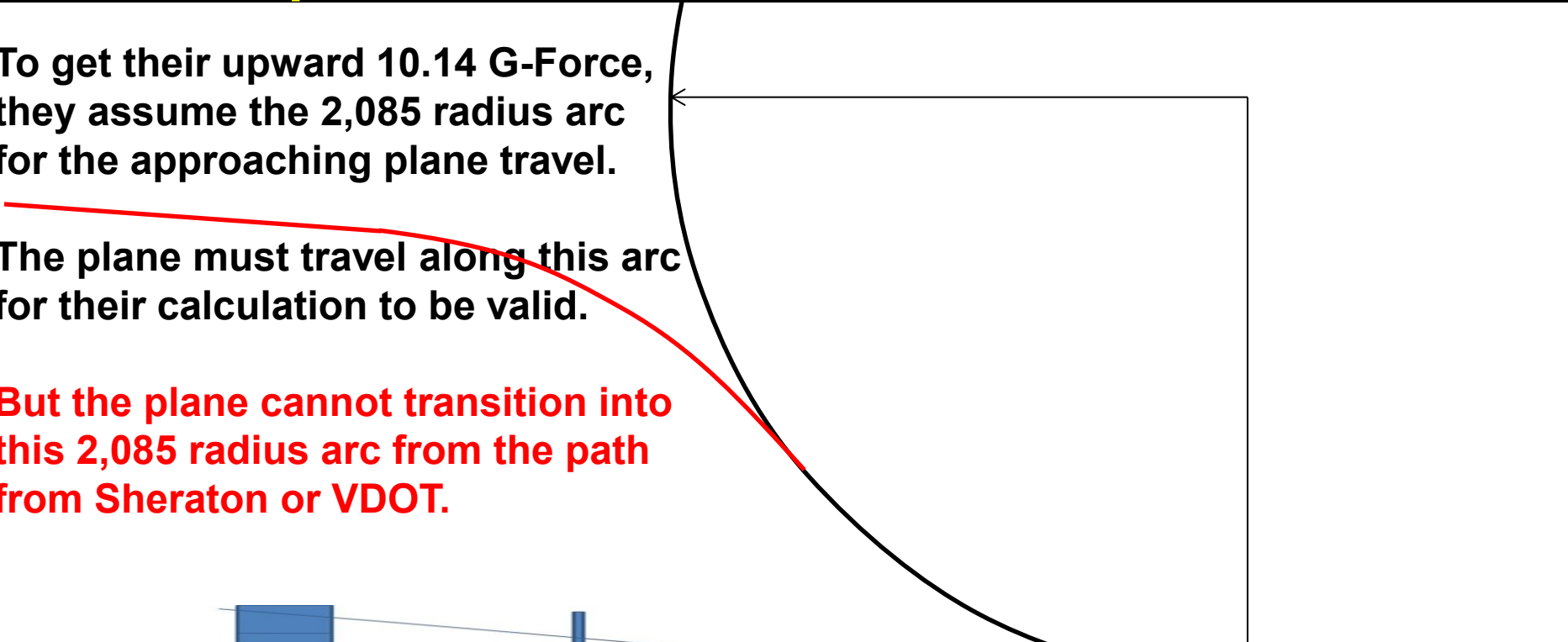
9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Pilots for 9/11 Truth Calculate Upward G-Forces of 10.14G

To get their upward 10.14 G-Force, they assume the 2,085 radius arc for the approaching plane travel.

The plane must travel along this arc for their calculation to be valid.

But the plane cannot transition into this 2,085 radius arc from the path from Sheraton or VDOT.



2,085 ft radius gives: 9 G-Force



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

Analysis of Pentagon Plane Approach and Impact

# APPENDIX: TRAILER IMPACT – BASIS FOR YAW ROTATION



A 501c(3) Non-Profit Educational Organization



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Plane Begins to Rotate

- The approaching plane has a weight of about 200 tons
  - Symmetric around line of travel
  - Engines are approximately 21 feet to either side of the centerline
- The generator trailer has a weight that is probably in the range of 20 - 40 tons
  - Probably 10 percent of the plane's mass



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Conservation of Momentum

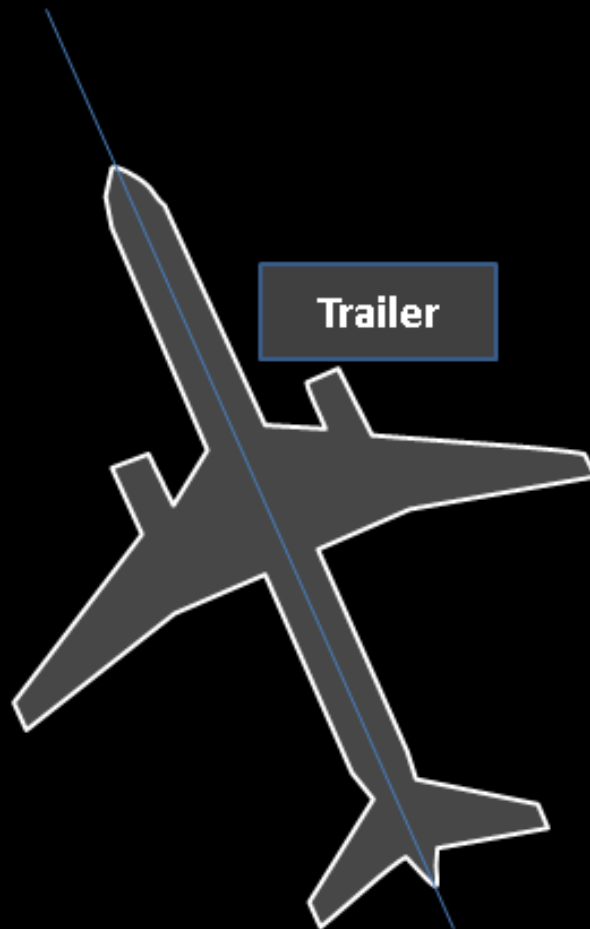
- Upon impact with the generator trailer
  - Significant force exerted on the trailer
  - Causes the trailer to accelerate toward the Pentagon wall
  - Rotates / moves about 15 feet



Because there is important news the “free press” isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017

# Plane Approaching



Because there is important news the "free press" isn't telling you

**9/11 and Other Deep State Crimes Teleconference** 95  
**October 25, 2017**

# Plane Transmits Energy to Trailer Creating Rotational Force to Plane



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State  
Crimes Teleconference  
October 25, 2017



# Rotation of Plane Around Center of Mass Continues



A 501c(3) Non-Profit Educational Organization



Because there is important news the "free press" isn't telling you

9/11 and Other Deep State Crimes Teleconference  
October 25, 2017